

RAIL TRAIL CONNECTION
NY Route 32 (N. Chestnut St.) and Henry W. Dubois Drive

Village of New Paltz
25 Plattekill Avenue
New Paltz, NY 12561
Ulster County, New York

Village Mayor: Tim Rogers
Deputy Mayor: Alexandria Wojcik
Trustees: William Wheeler-Murray
Stana Weisburd
Michele Zipp

April 12, 2023

Legal Notice Village of New Paltz

Please take notice that the Village Board of the Village of New Paltz is accepting bids from qualified contractors for the construction of a shared use path connection from the Empire State Trail along Henry W. Dubois Drive to the Wallkill Valley Rail Trail. The work includes two ADA compliant concrete curb ramps - one addition and one reconstruction, pedestrian signals with countdown timers, and re-striping along NY Route 32 to accommodate a new crosswalk. Additional Empire State Trail signage will be added to direct users to the appropriate location.

Plans and specifications are available from the Village of New Paltz Village Clerk. All handling costs (mail or delivery services) are the responsibility of the bidder. Contract documents may be examined at no expense at the office of the Village of New Paltz Village Clerk at 25 Plattekill Avenue, New Paltz, NY 12561 or requested from the Engineer via email at KristieDiCocco@altago.com.

The use of Minority & Women-Owned Business Enterprises is encouraged but not explicitly required.

Proposal Due:	5/2/2023 11:00AM
Contract Term:	12/22/2023
Submit To:	Village of New Paltz Village Clerk 25 Plattekill Avenue New Paltz, NY 12561 (845) 255-0130

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1 INTRODUCTION AND OVERVIEW

The Owner is the Village of New Paltz and the work site is located in the Village of New Paltz. The project limit includes the intersection of NY Route 32 (N. Chestnut St.) and Henry W. Dubois Drive.

This is a Village project sponsored by the Hudson River Valley Greenway. All work contemplated under this contract is to be covered by and in conformity with the latest revisions of the NYSDOT Standard Specifications (US Customary), which are current on the date of advertisement for bids, shall be considered in effect as posted on the New York State Department of Transportation's website.

2 PROJECT DESCRIPTION

The objective of this project is to provide a more direct route for Empire State Trail users to connect from the proposed Henry W. Dubois shared path to the existing Wallkill Valley Rail Trail (WVRT) at the newly reconstructed and signalized NY Route 32/Henry W. Dubois intersection. The current route requires bicyclists to cross NY Route 32 at Mulberry Street then use Church Street to connect to Henry W. Dubois. The proposed crossing will connect from the northeast quadrant of the NY Route 32/Henry W. Dubois intersection, directly across NY Route 32 to the WVRT. The scope of this connection will consist of two ADA compliant concrete curb ramps, a high visibility crosswalk and pedestrian signal heads with countdown timers.

The Rail Trail Access will connect the proposed project terminus of the Henry W. Dubois Drive Bike/Ped Project (PIN 8762.48) to the existing Wallkill valley Rail Trail. This connection is located on the northern leg of NY Route 32 (N. Chestnut St. / S.H. 572) and Henry W. Dubois Drive intersection and is approximately 40' in length.

The project seeks to provide a shared use path connection from the Empire State Trail along Henry W. Dubois Drive to the Wallkill Valley Rail Trail. This intersection improvement project at the intersection of NY Route 32 (N. Chestnut Street) and Henry W. Dubois Drive in the Village of New Paltz consists of the addition of one ADA compliant concrete curb ramp and reconstruction of one existing concrete curb ramp. Additional improvements include pedestrian signals with countdown timers, curb work, and re-striping along NY Route 32 to accommodate a new crosswalk. Additional Empire State Trail signage will be added to direct users to the appropriate location.

3 PROPOSAL DEADLINES

Proposals are due on May 2, 2023. The Village of New Paltz reserves the right to extend receipt of submissions beyond May 2, 2023.

4 SCOPE OF WORK

The following provides a general description of the scope of work. Schedule I provides more detail. Modifications to this scope of work by a Respondent to this RFP shall not be permitted unless approved by the Village of New Paltz

- two ADA compliant concrete curb ramps - one addition and one reconstruction
- a high visibility crosswalk
- pedestrian signal heads with countdown timers

5 SUBMITTAL CONTENT

Bids to be considered must be received in a sealed envelope at the office of the Village Clerk, Village of New Paltz, 25 Plattekill Ave, New Paltz, NY 12561 by 11:00 AM, local time, on May 2, 2023 at which time and place they will be publicly opened and read aloud. Bids received after the above noted time will not be accepted. All sealed envelopes should be clearly labeled "BID FOR WALLKILL VALLEY RAIL TRAIL CONNECTION PROJECT" and sent to:

Village Clerk
25 Plattekill Avenue

New Paltz, NY 12561
Telephone: 845.255.0130
E-Mail: Clerk@villageofnewpaltz.org

The sealed bids shall include the completed Bid Form, Non-Collusive Bidding Certification (as required by chapter 956 of the Laws of New York State), Bid Performance Bond, and Certificate of Insurance.

All bids must include the completed Bid Form. This is a unit price bid. The project will require a highway work permit with a performance bond in the amount of \$50,000. No bidder may withdraw their bid within forty-five (45) days after the actual date of opening thereof.

6 SELECTION PROCESS

The contract will, at the discretion of the Village, be awarded on the basis of competitive bids to the lowest responsible eligible bidder based on the Total Base Bid. Contractor shall be made aware that there has been a budget defined for this project. Should the bids come in over that budgeted amount, the Village is under no obligation to award the project.

7 SPECIFIC LEGAL OBLIGATIONS

7.1 PROCUREMENT 11

The Village of New Paltz is committed to providing all prospective respondents with accurate, consistent and timely information to ensure that the procurement is conducted with full and open competition. Written questions ONLY from prospective respondents about the RFP are accepted by mail or email no less than 3 business days before the proposal opening. Questions may only be addressed to the RFP Coordinator as identified in Section 5 (Submittal Content).

In accordance with General Municipal Law Section 104-b(2)(f) and State Finance Law Section 139-j(2)(a), the Village of New Paltz must identify the individual responsible for purchasing and the individual who is the sole point of contact during the procurement.

In accordance with State Finance Law Sections 139-j and 139-k, this RFP imposes restrictions on communications between the Village of New Paltz and Respondents during the procurement. Respondent is restricted from making contact from the earliest notice of intent to solicit offers through final award (the restricted period) with the Village of New Paltz's staff other than the RFP Coordinator unless it is a contact included among expressly provided statutory

exceptions set forth in State Finance Law Section 139-j(3)(a). Respondent is also restricted during this period from making contact with any employee of Hudson River Valley Greenway.

7.2 NEW YORK LAW AND VENUE

This contract shall be construed under the laws of the State of New York. All claims, actions, proceedings, and lawsuits brought in connection with, arising out of, related to, or seeking enforcement of this contract shall be brought in the Supreme Court of the State of New York, Rockland County.

SCHEDULE I: DETAILED PROJECT DESCRIPTION

Brought Forward \$_____.

WALLKILL VALLEY RAIL TRAIL CONNECTION PROJECT						
ITEM NUMBER	ITEM DESCRIPTION	PED/BIKE QTY	UNIT	UNIT PRICE IN WORDS	UNIT PRICE (NUMERALS)	PED/BIKE TOTAL (NUMERALS)
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	40	CY	Dollars	\$_____.	\$_____.
				Cents		
204.01	CONTROLLED LOW STRENGTH MATERIAL (CLSM)	10	CY	Dollars	\$_____.	\$_____.
				Cents		
206.03	CONDUIT EXCAVATION AND BACKFILL INCLUDING SURFACE RESTORATION	100	LF	Dollars	\$_____.	\$_____.
				Cents		
209.1703	DRAINAGE STRUCTURE INLET PROTECTION, PREFABRICATED-TEMPORARY	17	LF	Dollars	\$_____.	\$_____.
				Cents		
608.0101	CONCRETE SIDEWALKS AND DRIVEWAYS	4	CY	Dollars	\$_____.	\$_____.
				Cents		
608.21000003	CAST IRON EMBEDDED DETECTABLE WARNING UNITS	5	SY	Dollars	\$_____.	\$_____.
				Cents		

Brought Forward \$_____.

WALLKILL VALLEY RAIL TRAIL CONNECTION PROJECT						
ITEM NUMBER	ITEM DESCRIPTION	PED/BIKE QTY	UNIT	UNIT PRICE IN WORDS	UNIT PRICE (NUMERALS)	PED/BIKE TOTAL (NUMERALS)
609.0401	CAST-IN-PLACE CONCRETE CURB TYPE VF6	136	LF	<div>Dollars</div> <div>Cents</div>	<div>\$_____.</div>	<div>\$_____.</div>
610.1601	TURF ESTABLISHMENT - ROADSIDE	27	SY	<div>Dollars</div> <div>Cents</div>	<div>\$_____.</div>	<div>\$_____.</div>
625.01	SURVEY OPERATIONS	1	LS	<div>Dollars</div> <div>Cents</div>	<div>\$_____.</div>	<div>\$_____.</div>
627.50140008	CUTTING PAVEMENT	136	LF	<div>Dollars</div> <div>Cents</div>	<div>\$_____.</div>	<div>\$_____.</div>
635.0103	CLEANING AND PREPARATION OF PAVEMENT SURFACES - LINES	79	LF	<div>Dollars</div> <div>Cents</div>	<div>\$_____.</div>	<div>\$_____.</div>
645.81	TYPE A SIGN POSTS	3	EACH	<div>Dollars</div> <div>Cents</div>	<div>\$_____.</div>	<div>\$_____.</div>

Brought Forward \$_____.

WALLKILL VALLEY RAIL TRAIL CONNECTION PROJECT						
ITEM NUMBER	ITEM DESCRIPTION	PED/BIKE QTY	UNIT	UNIT PRICE IN WORDS	UNIT PRICE (NUMERALS)	PED/BIKE TOTAL (NUMERALS)
647.51	REMOVE AND DISPOSE SIGN PANEL, SIGN PANEL ASSEMBLY SIZE I(UNDER 30 SQUARE FEET)	2	EACH	<div>Dollars</div> <div>Cents</div>	\$_____.	\$_____.
680.5001	POLE EXCAVATION AND CONCRETE FOUNDATION	2	CY	<div>Dollars</div> <div>Cents</div>	\$_____.	\$_____.
680.510501	PULLBOX-RECTANGULAR, 26 X 18 INCH, REINFORCEDCONCRETE	1	EACH	<div>Dollars</div> <div>Cents</div>	\$_____.	\$_____.
680.520506	TRAFFIC SIGNAL CONDUIT, RIGID PLASTIC, CLASS 1, 2"	20	LF	<div>Dollars</div> <div>Cents</div>	\$_____.	\$_____.
680.520508	TRAFFIC SIGNAL CONDUIT, RIGID PLASTIC, CLASS 1, 3"	80	LF	<div>Dollars</div> <div>Cents</div>	\$_____.	\$_____.
680.670801	PEDESTRIAN SIGNAL POLE POST TOP MOUNT, 8 FEET OVERALL POLE HEIGHT STEEL	2	EACH	<div>Dollars</div> <div>Cents</div>	\$_____.	\$_____.

Brought Forward \$_____.

WALLKILL VALLEY RAIL TRAIL CONNECTION PROJECT						
ITEM NUMBER	ITEM DESCRIPTION	PED/BIKE QTY	UNIT	UNIT PRICE IN WORDS	UNIT PRICE (NUMERALS)	PED/BIKE TOTAL (NUMERALS)
680.730214	SIGNAL CABLE 2 CONDUCTORS, 14 AWG	300	LF	Dollars	\$_____.	\$_____.
				Cents		
680.730514	SIGNAL CABLE 5 CONDUCTORS, 14 AWG	285	LF	Dollars	\$_____.	\$_____.
				Cents		
680.81310109	ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITH POLE	1	EACH	Dollars	\$_____.	\$_____.
				Cents		
680.81310209	ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITHOUT POLE	1	EACH	Dollars	\$_____.	\$_____.
				Cents		
680.813105	PEDESTRIAN SIGNAL MODULE - 12 INCH BI- MODAL,HAND/MAN SYMBOLS LED	2	EACH	Dollars	\$_____.	\$_____.
				Cents		
680.8141	PEDESTRIAN SIGNAL BRACKET MOUNT ASSEMBLY	1	EACH	Dollars	\$_____.	\$_____.
				Cents		

Brought Forward \$_____.

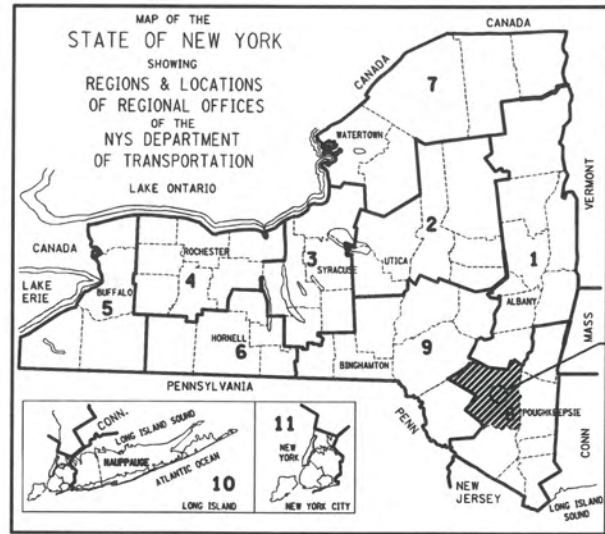
WALLKILL VALLEY RAIL TRAIL CONNECTION PROJECT						
ITEM NUMBER	ITEM DESCRIPTION	PED/BIKE QTY	UNIT	UNIT PRICE IN WORDS	UNIT PRICE (NUMERALS)	PED/BIKE TOTAL (NUMERALS)
680.8142	PEDESTRIAN SIGNAL POST TOP MOUNT ASSEMBLY	1	EACH	<div>Dollars</div> <div>Cents</div>	\$_____.	\$_____.
680.8207	OVERHEAD SIGN ASSEMBLY, TYPE G	1	EACH	<div>Dollars</div> <div>Cents</div>	\$_____.	\$_____.
685.11	WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES - 20 MILS	222	LF	<div>Dollars</div> <div>Cents</div>	\$_____.	\$_____.
688.01	WHITE PREFORMED REFLECTORIZED PAVEMENT STRIPES	25	LF	<div>Dollars</div> <div>Cents</div>	\$_____.	\$_____.
688.03	WHITE PREFORMED REFLECTORIZED PAVEMENT LETTERS	4	EACH	<div>Dollars</div> <div>Cents</div>	\$_____.	\$_____.
688.04	WHITE PREFORMED REFLECTORIZED PAVEMENT SYMBOLS	1	EACH	<div>Dollars</div> <div>Cents</div>	\$_____.	\$_____.

Page Total \$_____

Carry Forward \$_____

Brought Forward \$_____.

WALLKILL VALLEY RAIL TRAIL CONNECTION PROJECT						
ITEM NUMBER	ITEM DESCRIPTION	PED/BIKE QTY	UNIT	UNIT PRICE IN WORDS		PED/BIKE TOTAL (NUMERALS)
697.03	FIELD CHANGE PAYMENT	5000	DC	FIXED PRICE: SEE SPECIFICATION	Dollars	\$5,000.00
					Cents	\$5,000.00
	SUBTOTAL BASE BID				Dollars	\$_____.
					Cents	\$_____.
699.040001	MOBILIZATION	1	LS		Dollars	\$_____.
					Cents	\$_____.
	TOTAL BASE BID				Dollars	\$_____.
					Cents	\$_____.



PROJECT SITE



WALLKILL VALLEY RAIL TRAIL CONNECTION
VILLAGE OF NEW PALTZ
ULSTER COUNTY, NEW YORK

PREPARED FOR:
VILLAGE OF NEW PALTZ
25 PLATTEKILL AVENUE
NEW PALTZ, NEW YORK 12561

COUNTY ULSTER



PROJECT LOCATION
NOT TO SCALE

THIS PROJECT IS LOCATED IN THE VILLAGE OF NEW PALTZ IN
ULSTER COUNTY ON NYS ROUTE 32 FROM THE INTERSECTION WITH
AT HENRY W. DUBOIS DRIVE APPROX. 41°45'10"N 74°05'06"W.

THE LATEST REVISIONS OF THE STANDARD SHEETS MAINTAINED BY THE DEPARTMENT, WHICH ARE CURRENT ON THE DATE OF ADVERTISEMENT FOR BIDS, SHALL BE CONSIDERED TO BE IN EFFECT. ALL PAY ITEMS AND WORK CONTAINED IN THE CONTRACT AND ANY ADDITIONAL PAY ITEMS AND WORK ENCOUNTERED DURING THE COURSE OF THE CONTRACT SHALL BE SUBJECT TO THE APPLICABLE STANDARD SHEET(S) UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.

ALL WORK CONTEMPLATED UNDER THIS CONTRACT IS TO BE COVERED BY AND IN CONFORMITY WITH THE STANDARD SPECIFICATIONS (U.S. CUSTOMARY) REFERENCED IN THE CONTRACT PROJECT "PROPOSAL" EXCEPT AS MODIFIED BY THESE PLANS OR BY CHANGES SET FORTH IN THE CONTRACT PROJECT "PROPOSAL."

CONTRACT PLANS HAVE BEEN DESIGNED IN ACCORDANCE WITH NYSOT POLICIES AND GUIDELINES.

APPROVED BY

Kristie Di Cocco 4/7/23
KRISTIE DI COCCO, PE (NY# 088834) DATE
ALTA PLANNING + DESIGN, INC./H+Z
ENGINEERING AND LANDSCAPE ARCHITECTURE, D.P.C.

RECOMMENDED BY

VILLAGE OF NEW PALTZ MAYOR DATE

alta www.altago.com	1801 6th Avenue, Suite 204 Troy, NY 12180 518-874-4211	H+Z Engineering + Landscape Architecture, DPC
WALLKILL VALLEY RAIL TRAIL CONNECTION		
VILLAGE OF NEW PALTZ		
SUBMISSION: FINAL		
COUNTY: ULSTER		
FED. ROAD REG. NO.	STATE	SHEET NO.
1	N.Y.	1
INDEX ON SHEET NO. 2		




ALIGNMENT		TOPOGRAPHY (MISCELLANEOUS)		UTILITIES																																		
ABBR.	DESCRIPTION	ABBR.	DESCRIPTION	ABBR.	DESCRIPTION																																	
AH	AHEAD	ABUT	ABUTMENT	E	ELECTRIC																																	
AZ	AZIMUTH	AOBE	AS ORDERED BY ENGINEER	EMH	ELECTRIC MANHOLE																																	
BK	BACK	ASPH	ASPHALT	G	GAS																																	
Ⓢ	BASELINE	BDY	BOUNDARY	GP	GUY POLE																																	
BRG	BEARING	BLDG	BUILDING	GSB	GAS SERVICE BOX (HOUSE LINE)																																	
Ⓒ	CENTERLINE	BM	BENCH MARK	GV	GAS VALVE (MAIN LINE)																																	
CS	CURVE TO SPIRAL	CC	CENTER TO CENTER	HYD	HYDRANT																																	
e	SUPERELEVATION RATE (CROSS SLOPE)	CONC	CONCRETE	LP	LIGHT POLE																																	
EQ	EQUALITY	CONST	CONSTRUCTION	LPG	LOW PRESSURE GAS																																	
EXT	EXTERNAL	CR	COUNTY ROAD	PP	POWER POLE																																	
HCL	HORIZONTAL CONTROL LINE	D	DEED DISTANCE	SA	SANITARY SEWER																																	
HSD	HEADLIGHT SIGHT DISTANCE	DM	DIRECT MEASUREMENT	SMH	SANITARY MANHOLE																																	
L	LENGTH OF CIRCULAR CURVE	DWY	DRIVEWAY	ST	STORM SEWER																																	
LS	LENGTH OF SPIRAL	EP	EDGE OF PAVEMENT	T	TELEPHONE																																	
LVC	LENGTH OF VERTICAL CURVE	ES	EDGE OF SHOULDER	TCB	TRAFFIC CONTROL BOX																																	
E	CENTER CORRECTION OF VERTICAL CURVE	FEE	FEE ACQUISITION	TELBOX	TELEPHONE BOX																																	
M	MAIN LINE	FEE WO/A	FEE ACQUISITION WITHOUT ACCESS	TEL P	TELEPHONE POLE																																	
PC	POINT OF CURVATURE	FP	FENCE POST	TMH	TELEPHONE MANHOLE																																	
PI	POINT OF INTERSECTION	FD	FOUNDATION	CTV	CABLE TELEVISION																																	
POL	POINT ON LINE	FL	FENCE LINE	W	WATER																																	
PSD	PASSING SIGHT DISTANCE	GAR	GARAGE	WSB	WATER SERVICE BOX (HOUSE LINE)																																	
PT	POINT OF TANGENT	GR	GRAVEL	WV	WATER VALVE (MAIN LINE)																																	
PVC	POINT OF VERTICAL CURVE	HO	HOUSE	SUBSURFACE EXPLORATION																																		
PVI	POINT OF VERTICAL INTERSECTION	HWY	HIGHWAY																																			
PVT	POINT OF VERTICAL TANGENT	IP	IRON PIN OR IRON PIPE	ABBR.	DESCRIPTION																																	
R	RADIUS	MB	MAILBOX	REPLACE ABBREVIATION "AB" WITH:																																		
SC	SPIRAL TO CURVE	MON	MONUMENT	AH	HAND AUGER																																	
SSD	STOPPING SIGHT DISTANCE	N&W	NAIL AND WASHER	CP	CONE PENTROMETER																																	
ST	SPIRAL TO TANGENT	OG	ORIGINAL GROUND	DA	2¼ INCHES CASED DRILL HOLE																																	
STA	STATION	O/H	OVERHEAD	DM	DRILLING MUD																																	
T	TANGENT LENGTH	P	PARCEL	DN	4 INCHES CASED DRILL HOLE																																	
TGL	THEORETICAL GRADE LINE	PAV'T	PAVEMENT	FH	HOLLOW FLIGHT AUGER																																	
TS	TANGENT TO SPIRAL	PE	PERMANENT EASEMENT	PA	POWER AUGER																																	
VC	VERTICAL CURVE	PED POLE	PEDESTRIAN POLE	PH	PROBE																																	
TOPOGRAPHY (DRAINAGE)		Ⓘ	PROPERTY LINE	PT	PERCOLATION TEST HOLE																																	
		POR	PORCH	RP	1 INCH SAMPLER (RETRACTABLE PLUG)																																	
ABBR.	DESCRIPTION	RR	RAILROAD	TO BE DEFINED AT THE TIME OF EXPLORATION																																		
BB	BOTTOM OF BANK (STREAM)	RTE	ROUTE	SP	SEISMIC POINT																																	
BC	BOTTOM OF CURB	ROW	RIGHT OF WAY	TP	TEST PIT																																	
BO	BOTTOM OF OPENING	RW	RETAINING WALL	ABBREVIATION "C" IN CATEGORIES: DA, DM, DN, AND FH WITH:																																		
CAP	CORRUGATED ALUMINUM PIPE	SH	STATE HIGHWAY	B	BRIDGE																																	
CB	CATCH BASIN	SHLDR	SHOULDER	C	CUT																																	
CIP	CAST IRON PIPE	SPK	SPIKE	D	DAM																																	
Ⓒ STRM	CENTERLINE OF STREAM	ST	STREET	F	FILL																																	
CMP	CORRUGATED METAL PIPE	STK	STAKE	K	CULVERT																																	
CP	CONCRETE PIPE	STY	STORY	W	WALL																																	
CSP	CORRUGATED STEEL PIPE	SW	SIDEWALK	X	TO BE USED IF ONE OF THE ABOVE CANNOT BE DEFINED AT THE TIME THE EXPLORATION IS MADE																																	
CULV	CULVERT	TE	TEMPORARY EASEMENT																																			
DIA	DIAMETER	TO	TEMPORARY OCCUPANCY																																			
DMH	DRAINAGE MANHOLE	U/G	UNDERGROUND																																			
DS	DRAINAGE STRUCTURE PIPE	WW	WING WALL																																			
D'XING	DITCH CROSSING	<table><tr><th>STANDARD SYMBOL (PLANS)</th><th>ITEM PAYMENT UNIT: ESTIMATE OF QUANTITIES SHEET</th><th>EQUIVALENT NOMENCLATURE: (SPECS/PROPOSAL)</th></tr><tr><td>"</td><td>-</td><td>INCHES</td></tr><tr><td>'</td><td>LF</td><td>LINEAR FEET</td></tr><tr><td>mi</td><td>MI</td><td>MILES</td></tr><tr><td>ft²</td><td>SF</td><td>SQUARE FEET</td></tr><tr><td>YD²</td><td>SY</td><td>SQUARE YARD</td></tr><tr><td>AC</td><td>AC</td><td>ACRES</td></tr><tr><td>YD³</td><td>CY</td><td>CUBIC YARD</td></tr><tr><td>GAL</td><td>GAL</td><td>GALLON</td></tr><tr><td>lb</td><td>LB</td><td>POUND</td></tr><tr><td>TON</td><td>TON</td><td>TON</td></tr></table>				STANDARD SYMBOL (PLANS)	ITEM PAYMENT UNIT: ESTIMATE OF QUANTITIES SHEET	EQUIVALENT NOMENCLATURE: (SPECS/PROPOSAL)	"	-	INCHES	'	LF	LINEAR FEET	mi	MI	MILES	ft²	SF	SQUARE FEET	YD²	SY	SQUARE YARD	AC	AC	ACRES	YD³	CY	CUBIC YARD	GAL	GAL	GALLON	lb	LB	POUND	TON	TON	TON
STANDARD SYMBOL (PLANS)	ITEM PAYMENT UNIT: ESTIMATE OF QUANTITIES SHEET					EQUIVALENT NOMENCLATURE: (SPECS/PROPOSAL)																																
"	-					INCHES																																
'	LF					LINEAR FEET																																
mi	MI					MILES																																
ft²	SF					SQUARE FEET																																
YD²	SY					SQUARE YARD																																
AC	AC					ACRES																																
YD³	CY					CUBIC YARD																																
GAL	GAL					GALLON																																
lb	LB					POUND																																
TON	TON					TON																																
EHW	EXTREME HIGH WATER																																					
EL	ELEVATION																																					
ELEV	ELEVATION																																					
ELW	EXTREME LOW WATER																																					
ES	END SECTION																																					
HW	HEADWALL																																					
INV	INVERT																																					
MH	MANHOLE																																					
MHW	MEAN HIGH WATER																																					
OHW	ORDINARY HIGH WATER																																					
OLW	ORDINARY LOW WATER																																					
RCP	REINFORCED CONCRETE PIPE																																					
SICPP	SMOOTH INTERIOR CORRUGATED POLYETHYLENE PIPE																																					
TB	TOP OF BANK (STREAM)																																					
TC	TOP OF CURB																																					
TG	TOP OF GRATE																																					
VCP	VITRIFIED CLAY PIPE																																					

WALLKILL VALLEY RAIL TRAIL CONNECTION

INDEX		TOTAL NUMBER OF SHEETS
SHEET NUMBER	DESCRIPTION	DRAWING NUMBER
1	TITLE SHEET	COVER
2	INDEX AND ABBREVIATIONS	INDEX
3-4	LEGEND, LINE AND POINT SYMBOLOGY	LEG-1 TO LEG-2
5	TYPICAL SECTION	TYP-1
6	GENERAL NOTES	GNN-1
7	MISCELLANEOUS DETAILS	MSD-01
8	GENERAL PLAN	GNP-01
9	TRAFFIC SIGNAL NOTES	TSN-01
10	TRAFFIC SIGNAL PLAN	TSP-01

STANDARD SHEETS

209-03
608-01
609-01
619-10
619-11
619-12
619-080
619-310
619-410
619-422
619-520
645-03
680-01
680-02
680-06
680-10
680-13
680-17
685-01

WALLKILL VALLEY RAIL TRAIL CONNECTION		PIN	BRIDGES	CULVERTS	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED		CONTRACT NUMBER				
					INDEX AND ABBREVIATIONS		DRAWING NO. INDEX SHEET NO. 2				
VILLAGE OF NEW PALTZ		REGION: 8									
COUNTY: ULSTER											
				 www.altago.com		1801 6th Avenue, Suite 204 Troy, NY 12180 518-874-4211		 Engineering + Landscape Architecture, LLC		 VILLAGE OF NEW PALTZ	

ALIGNMENT			LANDSCAPE			ROADWAY			TRAFFIC WORK ZONE		
STYLE	NAME	DESCRIPTION	STYLE	NAME	DESCRIPTION	STYLE	NAME	DESCRIPTION		TWZBT_P	BARRIER, TEMPORARY
	AC	CONTROL (CENTERLINE)		LABL	AREA, BRUSH LINE		RCZ_P	CLEAR ZONE		TWZBTWL_P	BARRIER, TEMPORARY, W/ WARNING LIGHTS
	AD_P	DETOUR		LAHR	AREA, HEDGE ROW		RG	GUIDE RAIL, MISCELLANEOUS		TWZCD_P	CHANNELIZING DEVICE
	AT_P	TRANSITION CONTROL		LAPB	AREA, PLANTING BED		RGB	GUIDE RAIL, BOX BEAM		TWZPMRC_P	PAVEMENT MARKING REMOVAL OR COVERING
BRIDGE				LAWA	AREA, WOODED AREA OUTLINE		RGBM	GUIDE RAIL, BOX BEAM, MEDIAN	UTILITIES		
	BR	RAIL		LAWL	AREA, WATERS EDGE		RGC	GUIDE RAIL, CABLE	STYLE	NAME	DESCRIPTION
	BSHT	SHEET PILING		LCUT_P	CUT LIMIT		RGCB	GUIDE RAIL, CONCRETE BARRIER		UC	CONDUIT, UNDERGROUND
CONTROL				LFILL_P	FILL LIMIT		RGP_P	GUIDE POST		UCH	CONDUIT, HANGING
	CB	BASELINE		LFNC	FENCE		RGW	GUIDE RAIL, W BEAM		UCO	CONDUIT, OVERHEAD
	CBPR	BASELINE, PROJECTION		LTRC	TREE ROW, CONIFEROUS		RGWM	GUIDE RAIL, W BEAM, MEDIAN		UE	ELECTRIC LINE, UNDERGROUND
DRAINAGE				LTRD	TREE ROW, DECIDUOUS		RPB	PARKING BUMPER		UEH	ELECTRIC LINE, HANGING
	DCP	CULVERT PIPE		LWH	WALL, H PILE		RRC	RAIL ROAD, CATENARY		UEO	ELECTRIC LINE, OVERHEAD
	DCP_P	CULVERT PIPE (DIR)		LWR	WALL, RETAINING		RRER	RAIL ROAD, 3RD RAIL		UETO	ELECTRIC TRANSMISSION, OVERHEAD
	DDG_P	DITCH, GRASS LINED		LWS	WALL, STONE		RRPLS_P	RAIL, PHOTO, LARGE SCALE		UESS	ELECTRIC, SUBSTATIONS
	DDP_P	DITCH, PAVED INVERT	ROW MAPPING				RRPSS	RAIL, PHOTO, SMALL SCALE		UF0	FIBER OPTIC, UNDERGROUND
	DDS_P	DITCH, STONE LINED		MDL	DEED LINE		RRS	RUMBLE STRIP		UFOH	FIBER OPTIC, HANGING
	DFL_P	FLOW LINE		MEE	EASEMENT, EXISTING		RRSLS_P	RAIL, SURVEY, LARGE SCALE		UFOO	FIBER OPTIC, OVERHEAD
	DSSD	SLOTTED DRAIN		MEP_P	EASEMENT, PERMANENT		RRSSS	RAIL, SURVEY, SMALL SCALE		UG	GAS, UNDERGROUND
	DUD_P	UNDERDRAIN		MEPA_P	EASEMENT, PERMANENT, APPROX.	SIGNS				UGH	GAS, HANGING
ENVIRONMENTAL				MET_P	EASEMENT, TEMPORARY	SIGN				UGO	GAS, OVERHEAD
	ECT	CURTAIN, TURBIDITY		META_P	EASEMENT. TEMPORARY, APPROX.		SBLB	BILLBOARDS		UIC	INFORM CABLE, UNDERGROUND
	EDMC	DAM, COFFER		MF_P	FEE ACQUISITION, W/ ACCESS		SM	MULTIPLE POST		UICH	INFORM CABLE, HANGING
	EDMEC_P	DAM, EARTHEN CHECK		MFA_P	FEE ACQUISITION, APPROXIMATE		SSO	STRUCTURE, OVERHEAD		UO	OIL LINE, UNDERGROUND
	EDMGSC_P	DAM, GRAVEL BAG/SAND BAG CHECK		MFS_P	FEE ACQUISITION, SHAPE		SSOC	STRUCTURE, OVHD. CANTILEVER		UOH	OIL LINE, HANGING
	EDMPC_P	DAM, PREFABRICATED CHECK		MFWOA_P	FEE ACQUISITION, W/O ACCESS	STRIPING				UPBP	POLE, BRACE, PUSH BRACE
	EDMSC_P	DAM, STONE CHECK		MHA	HISTORICAL, ACQUISITION		STB*	BROKEN LINE		UPGW	POLE, GUY WIRE
	EFNS	FENCE, SILT		MHB	HIGHWAY BOUNDARY		STDB*	DOUBLE BROKEN LINE		USA	SANITARY SEWER, UNDERGROUND
	EFNSV	FENCE, SILT & VEGETATION		MHBW	HWY BOUNDARY, FACE OF WALL		STD*	DOTTED LINE LONG		USAH	SANITARY SEWER, HANGING
	EFNV	FENCE, VEGETATION		MHBWOA	HIGHWAY BOUNDARY, W/O ACCESS		STDL*	DOTTED LINE SHORT		USAF	SANITARY SEWER, FORCE MAIN, UGND
	ESFL	FILTER, SEDIMENT LOG		MJC	JURISDICTION, CITY		STDS*	DOTTED LINE SHORT		USAFH	SANITARY SEWER, FORCE MAIN, HANG
	EWAA_P	WETLAND, ADJACENT AREA		MJCY	JURISDICTION, COUNTY		STFB*	FULL BARRIER LINE		UT	TELEPHONE, UNDERGROUND
	EFW	WETLAND, FEDERAL		MJHD	JURISDICTION, HISTORIC DISTRICT		STH*	HATCH LINE		UTH	TELEPHONE, HANGING
	EFWS	WETLAND, FEDERAL AND STATE		MJLL	JURIS., (GREAT, MILITARY) LOT LINE		STPB*	PARTIAL BARRIER LINE		UTO	TELEPHONE, OVERHEAD
	EWM	WETLAND, MITIGATION AREA		MJN	JURISDICTION, NATION		STRCT	ROUNDABOUT, CAT TRACKS		UTV	CABLE TV, UNDERGROUND
	EWS	WETLAND, STATE		MJPB	JURISDICTION, PUBLIC LANDS		STRYL	ROUNDABOUT, YIELD LINE		UTVH	CABLE TV, HANGING
				MJS	JURISDICTION, STATE		STSB	STOP BAR		UTVO	CABLE TV, OVERHEAD
				MJT	JURISDICTION, TOWN		STSE*	SOLID, EDGE		UUJ	UNKNOWN, UNDERGROUND
				MJV	JURISDICTION, VILLAGE		STXL	X WALK, LADDER LINE		UUH	UNKNOWN, HANGING
				MPL	PROPERTY LOT LINE		STXLB	X WALK, LADDER BAR LINE		UUO	UNKNOWN, OVERHEAD
				MPLA	PROPERTY LOT LINE, APPROXIMATE	* = W (WHITE) OR Y (YELLOW)				UW	WATER LINE, UNDERGROUND
				MSL	SUB LOT LINE	TRAFFIC CONTROL				UWH	WATER LINE, HANGING
						TCSW	SIGNAL, SPAN WIRE		UWO	WATER LINE, OVERHEAD	

1. THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED).
2. FEATURES ARE SHOWN AS EITHER LINEAR (ROADWAY GUIDERAIL, ROADWAY SIDEWALK, UTILITY LINES, ETC.) OR POINT (SIGN, UTILITY POLE, ETC.).
3. FEATURES SHOWN ON THE LEGEND AS EXISTING FEATURES ALSO HAVE CORRESPONDING PROPOSED FEATURES.
4. PROPOSED FEATURE SYMBOLOGY IS IDENTICAL TO EXISTING FEATURE SYMBOLOGY EXCLUDING LINE WEIGHT. LINE WEIGHT FOR PROPOSED FEATURES IS THICKER (0.015 in ON B SIZE DRAWINGS).
5. MAPPING FEATURES NOT INCLUDED ON THE LEGEND SHEET DO NOT HAVE A UNIQUE SYMBOLOGY (SUCH AS THE PAVEMENT EDGE, PAVEMENT EDGE OF TRAVEL WAY) AND SHOULD BE LABELED ON THE PLANS.
6. FEATURES SHOWN AT THE HEAVIER WEIGHT ARE PROPOSED ONLY AND DO NOT HAVE CORRESPONDING EXISTING FEATURES.

WALLKILL VALLEY RAIL TRAIL CONNECTION		PIN	BRIDGES	CULVERTS	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED	CONTRACT NUMBER				
VILLAGE OF NEW PALTZ						DRAWING NO. LEG-01				
COUNTY: ULSTER		REGION: 8		LEGEND - LINE SYMBOLOGY						
				SHEET NO. 3						
					1801 6th Avenue, Suite 204 Troy, NY 12180 518-874-4211					
					VILLAGE OF NEW PALTZ					

ALIGNMENT			DRAINAGE			ITS			ROW MAPPING			SIGNS			UTILITIES		
CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION
	ACC	CENTER OF CURVATURE		DINV	INVERT		IANT_P	ANTENNAS		MDL1P	DEED LINE, TYPE 1		S	SINGLE POST		UEB	ELECTRIC, BOX
	ACOGO	COGO		DS	STRUCTURE, RECTANGULAR		IASCTS	ACCOU. SPEED/COUNT SNSR.S		MDL2P	DEED LINE, TYPE 2		S_P	SINGLE POST, PROPOSED		UEM	ELECTRIC, METER
	ACS	CURVE TO SPIRAL		DSI	STRUCTURE, INVERT		ICABPAD	CABINET & PAD		MDL3P	DEED LINE, TYPE 3		SB_P	BACK TO BACK, PROPOSED		UEMH	ELECTRIC, MANHOLE
	ADPI_P	DETOUR, POINT OF INTERSECT.		DSM	STRUCTURE, MANHOLE		ICCTV	CCTV SITE		MDL4P	DEED LINE, TYPE 4		SDEL	DELINEATORS		UEPT	ELECTRIC, POLE, TRANS.
	ADPL_P	DETOUR, POINT ON LINE		DSMTXX_P	STRUCTURE, MANHOLE, TYPE "XX" "XX" = 48, 60, 72, 96		ICDPD	CDPD TRANSCEIVER		MDL5P	DEED LINE, TYPE 5		SPM	PARKING METER		UGM	GAS, METER
	AEQN	EQUATION		DSR	STRUCTURE, ROUND		ICELLT	CELL PHONE TOWER		MEEP	EASEMENT, EXISTING		SRM	REFERENCE MARKERS		UGMH	GAS, MANHOLE
	AEQNAHD	EQUATION AHEAD		DST	STRUCTURE, RECT., WITH CURB TYPE "X" "X" = F, G, N, O, P, R		ICJB	CONDUIT JACK OR BORING		MEPAP_P	EASEMENT, PERM., APPROX.		SRSC3	SHLD, CTY, 123 DIG.		UGLM	GAS, LINE MARKER
	AEQNBK	EQUATION BACK		DST	STRUCTURE, RECT., WITH CURB TYPE "X" "X" = F, G, N, O, P, R		ICNTLCAB	CONTROLLER CABINET		MEPP_P	EASEMENT, PERM., BACK LINE		SRSC4	SHLD, CTY, 4 DIG.		UGP	GAS/FUEL PUMP
	AEVT	EVENT STATION		DST	STRUCTURE, RECT., TYPE "X" "X" = I, K, L, M, O, P, U		ICPB	COMMUNICATION PULL BOX		MEPSP_P	EASEMENT, PERM., SHAPE		SRSCT2	SHLD, CTY TOUR, 1-2 DIG.		UGV	GAS, VALVE
	APC	POINT OF CURVATURE		DST	STRUCTURE, RECT., TYPE "X" "X" = I, K, L, M, O, P, U		ICTD	CONDUIT TURNING DOWN		MFAP_P	FEE ACQUISITION, APPROX.		SRSCT4	SHLD, CTY TOUR, 3-4 DIG.		UGVT	GAS, VENT
	APCC	POINT OF COMPOUND CURVATURE	ENVIRONMENTAL				ICTU	CONDUIT TURNING UP		MFP_P	FEE ACQUISITION, BACK LINE		SRSI	SHLD, INTERSTATE		ULP	LIGHTING, POLE
	API	POINT OF INTERSECTION					ICVTRT	COMM. VEH. ROAD TRANSCEIVER		MFSP_P	FEE ACQUISITION, SHAPE		SRSN2	SHLD, NATIONAL, 2 DIG.		ULPM	LIGHTING, POLE, MEDIAN
	APOB	POINT OF BEGINNING					IDEFAULT	DEFAULT		MHBAP	HIGHWAY BNDRY., APPROX.		SRSN3	SHLD, NATIONAL, 3 DIG.		ULPP	LIGHTING, POLE, PED.
	APOC	POINT OF CURVATURE					IEZR	E-ZPASS READER		MHBCP	HISTORICAL, BLDG. CORNERS		SRSS2	SHLD, STATE, 2 DIG.		UMFC	MISC. FILLER CAP
	APOE	POINT OF END					IEZTR	TRANSMITTAL READER		MHBP	HIGHWAY BNDRY, PT.		SRSS3	SHLD, STATE, 3 DIG.		UOLM	OIL, LINE MARKER
	APOL	POINT ON LINE		IFOXCAB	FIBER OPTIC X-CONNECT CABINET		MJCP	PT., JURIS. CITY		SRSS4	SHLD, STATE, 4 DIG.		UP	POLE, WITH UTILITY			
	APOS	POINT ON SPIRAL		IFUSSPL	FUSION SPLICE		MPBC	PT., BUILDING CORNER	TRAFFIC CONTROL					UPD	POLE, DEAD (NO UTILITY)		
	APOT	POINT ON TANGENT		IHARADV	HAR ADVISORY SIGN		MPCC	PT., CROSS CUT						UPL	POLE, WITH LIGHT		
	APOVC	POINT ON VERTICAL CURVE		IHARST	HAR SITE		MPDH	PT., DRILL HOLE						USMH	SANITARY SEWER MANHOLE		
	APOVT	POINT ON VERTICAL TANGENT		ILC	LOAD CENTER		MPF	PT., FENCE LOCATION						UTB	TELEPHONE, BOOTH		
	APORC	POINT ON REVERSE CURVE		IMECSPL	MECHANICAL SPLICE		MPIP	PT., IRON PIPE						UTLM	TELEPHONE, LINE MARKER		
	APT	POINT OF TANGENCY		IMSCS	PORT. SPEED & COUNT SENSOR		MPIR	PT., IRON ROD	TRAFFIC WORK ZONE					UTMH	TELEPHONE, MANHOLE		
	APVC	POINT OF VERTICAL CURVATURE		IMSCTS	MICRO SPEED & COUNT SENSOR		MPM	PT., MONUMENT						UTVLM	CABLE TV, LINE MARKER		
	APVCC	POINT OF VERT. CMPND CURVE		IMT	MICROWAVE TRANSCEIVER		MPMM	PT., MONUMENT, MISC.						UTVPB	CABLE TV, PULL BOX		
	APVI	POINT OF VERT. INTERSECTION		IOVHVS	PERM. OVERHEAD VMS		MPN	PT., NAIL						UUB	UNKNOWN, BOX		
	APVRC	POINT OF VERT. REVERSE CURVE		IPASCS	PORT. ACCOU. SPD & CNT. SENSOR		MPRS	PT., RAILROAD SPIKE					TRAFFIC WORK ZONE				
	APVT	POINT OF VERTICAL TANGENCY		IPEDS	PEDESTRIAN SIGNAL HEAD		MPST	PT., SPIKE		UUMH	UNKNOWN, MANHOLE						
	ASC	SPIRAL TO CURVE		IPSS	PAVEMENT SURFACE SENSOR		MPTW	PT., STAKE		UUPB	UNKNOWN, PULL BOX						
	ASPI	SPIRAL POINT OF INTERSECTION		IPVMS	PERM. VMS		MPWL	PT., TREE W/ WIRE		UUVL	UNKNOWN, VALVE						
	ASTS	SPIRAL TO SPIRAL		IRM	RAMP METER		MPSWL	PT., WALL LOCATION		UUVT	UNKNOWN, VENT						
	AST	SPIRAL TO TANGENT		IRWIS	RDWY WEATHER INFO. SENSOR	ROW ACQUISITION				TWZCMS_P	CHANGEABLE MESSAGE SIGN (PVMS)		UJW	UNKNOWN, WELL			
	ATS	TANGENT TO SPIRAL		ISP	SOLAR PANEL					TWZFLG_P	FLAGGER		UWFH	WATER, FIRE HYDRANT			
	AVEVT	VERTICAL EVENT POINT		ISST	SPREAD SPECT. TRANSCEIVER					TWZFT_P	FLAG TREE		UWM	WATER, METER			
	AVHIGH	VERTICAL HIGH POINT		ITDB	TELEPHONE DEMARCATION BLK					TWZIA_P	IMPACT ATTENUATOR / CRASH CUSHION (TEMPORARY)		UWMH	WATER, MANHOLE			
	AVLOW	VERTICAL LOW POINT		ITP	SUBSURFACE TEMP. PROBE					TWZLUM_P	LUMINAIRE (TEMPORARY)		UVV	WATER, VALVE			
BRIDGE				LTC	TREE, CONIFEROUS		IVTRT	VEHICLE TO RDWY TRANSCEIVER		TWZSDT_P	SYMBOL, DIRECTION OF TRAFFIC		UTVLM	CABLE TV, LINE MARKER			
				LTD	TREE, DECIDUOUS		IWIMD	WEIGHT IN MOTION DETECTOR		TWZSDTD_P	SYMBOL, DIRECTION OF TEMPORARY TRAFFIC DETOUR		UTVPB	CABLE TV, PULL BOX			
				LTS	TREE, STUMP		IWVRC	WIRELESS VIDEO REPEATER		TWZSGN_P	SIGN (TEMPORARY)		UUB	UNKNOWN, BOX			
				LTW_P	TREE, WELL OR WALL		IWVR	WIRELESS VIDEO RECEIVER		TWZSIG_P	SIGNAL, TRAFFIC OR PEDESTRIAN (TEMPORARY)		UJJB	UNKNOWN, JUNCTION BOX			
				LUKP	UNKNOWN POINT		IWVTT	WIRELESS VIDEO TRANSMITTER		TWZWL_P	WARNING LIGHT		UUMH	UNKNOWN, MANHOLE			
CONTROL			1. THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED).														
			2. FEATURES ARE SHOWN AS EITHER LINEAR (ROADWAY GUIDERAIL, ROADWAY SIDEWALK, UTILITY LINES, ETC.) OR POINT (SIGN, UTILITY POLE, ETC.).														
			3. FEATURES SHOWN ON THE LEGEND AS EXISTING FEATURES ALSO HAVE CORRESPONDING PROPOSED FEATURES.														
			4. PROPOSED FEATURE SYMBOLOGY IS IDENTICAL TO EXISTING FEATURE SYMBOLOGY EXCLUDING LINE WEIGHT. LINE WEIGHT FOR PROPOSED FEATURES IS THICKER (0.015 in ON B SIZE DRAWINGS).														
			5. MAPPING FEATURES NOT INCLUDED ON THE LEGEND SHEET DO NOT HAVE A UNIQUE SYMBOLOGY (SUCH AS THE PAVEMENT EDGE, PAVEMENT EDGE OF TRAVEL WAY) AND SHOULD BE LABELED ON THE PLANS.														
	CBP	BASELINE, POINT	LEGEND - POINT SYMBOLOGY														
	CBPOL	BASELINE, POINT ON LINE															
	CBSP	BASELINE, SPUR POINT															
	CBTP	BASELINE, TIE POINT															
	CPBM	BENCHMARK															
	CPH	POINT, HORIZ. PHOTOGRAMMETRY	LEGEND - POINT SYMBOLOGY														
	CPSM	POINT, SURVEY MARKER, PERM.															
	CPSV	POINT, VERT., PHOTOGRAMMETRY															

WALLKILL VALLEY RAIL TRAIL CONNECTION		PIN	BRIDGES	CULVERTS	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED	CONTRACT NUMBER

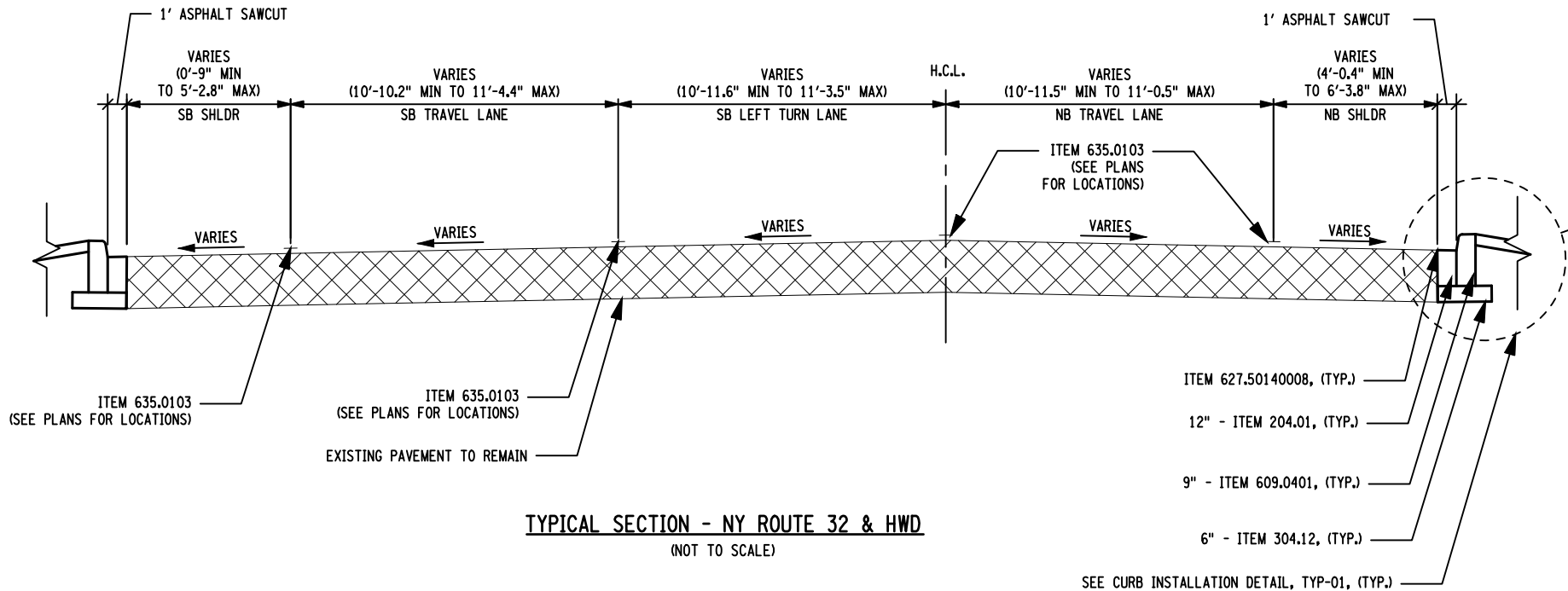
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DATE/TIME = 4/7/2023 5:04 PM
USER = MIKEREPSCH




PROJECT MANAGER K. DI COCCO
CHECK K. DI COCCO
DRAFTING D. JLELATY
CHECK K. DI COCCO
DESIGN D. JLELATY
JOB MANAGER K. DI COCCO
DESIGN SUPERVISOR K. DI COCCO

AFFIX SEAL:
ON: 04/07/2023

ALTERED BY:
ON:





ITEM	DESCRIPTION	UNIT	ITEM	DESCRIPTION	UNIT	NOTES:					
204.01	CONTROLLED LOW STRENGTH MATERIAL (CLSM)	CY				1. CONTRACTOR TO REMOVE EXISTING ASPHALT PAVEMENT TO THE TOP OF EXISTING SUBBASE. 2. SAWCUT LIMITS VARY. SAWCUT LIMITS CAN BE FOUND ON THE GENERAL PLANS GNP-01.					
304.12	SUBBASE COURSE, TYPE 2	CY									
608.0101	CONCRETE SIDEWALKS AND DRIVEWAYS	CY									
609.0401	CAST-IN-PLACE CONCRETE CURB TYPE VF6	LF									
627.50140008	CUTTING PAVEMENT	LF									
635.0103	CLEANING AND PREPARATION OF PAVEMENT SURFACES - LINES	LF									
AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:		WALLKILL VALLEY RAIL TRAIL CONNECTION		PIN	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED		CONTRACT NUMBER		
							TYPICAL SECTION SHEET		DRAWING NO. TYP-01 SHEET NO. 5		
		VILLAGE OF NEW PALTZ									
COUNTY: ULSTER		REGION: 8									
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						 1801 6th Avenue, Suite 204 Troy, NY 12180 518-874-4211 www.altago.com		 H+Z Engineering + Landscape Architecture, LLC		 VILLAGE OF NEW PALTZ	

alta
www.altago.com
1801 6th Avenue, Suite 204
Troy, NY 12180
518-874-4211

H+Z
Engineering & Landscape
Architecture, LLC



VILLAGE OF
NEW PALTZ

GENERAL NOTES

1. MATERIAL AND CONSTRUCTION SPECIFICATIONS: "STANDARD SPECIFICATIONS, CONSTRUCTION AND MATERIALS". NEW YORK STATE DEPARTMENT OF TRANSPORTATION (NYSOT OFFICE OF ENGINEERING, DATED JANUARY 1, 2023 WITH CURRENT ADDITIONS AND MODIFICATIONS, SHALL BE IN EFFECT FOR THIS PROJECT.
2. CURRENT NATIONAL "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) WITH NEW YORK STATE SUPPLEMENT SHALL BE IN EFFECT FOR THIS PROJECT.
3. ADDITIONAL NOTES MAY BE FOUND ON SUBSEQUENT DRAWINGS. SUCH NOTES, WHILE PERTAINING TO THE SPECIFIC DRAWING THEY ARE PLACED ON, ALSO SUPPLEMENT THE GENERAL NOTES LISTED HEREIN.
4. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT DUE TO THE NATURE OF TRAIL PROJECTS, THE EXACT EXTENT OF THE WORK CANNOT ALWAYS BE ACCURATELY DETERMINED PRIOR TO THE COMMENCEMENT. THESE CONTRACT DOCUMENTS HAVE BEEN PREPARED BASED ON FIELD INSPECTION AND OTHER INFORMATION AVAILABLE AT THE TIME. ACTUAL FIELD CONDITIONS MAY REQUIRE MODIFICATIONS TO CONSTRUCTION DETAILS AND WORK QUANTITIES. THE CONTRACTOR SHALL PERFORM THE WORK IN ACCORDANCE WITH THE CONDITIONS AND A.O.B.E.
5. THE CONTRACTOR SHALL EXAMINE AND VERIFY IN THE FIELD ALL EXISTING CONDITIONS AND DIMENSIONS WITH THOSE SHOWN ON THE PLANS. THE CONTRACTOR SHALL USE THE FIELD CONDITIONS AND DIMENSIONS, AND MAKE THE APPROPRIATE CHANGES TO THOSE SHOWN ON THE PLANS AS A.O.B.E. THE RESULTS OF THIS CHECK OF CONDITIONS AND DIMENSIONS SHALL BE SO NOTED ON THE DRAWINGS SUBMITTED FOR APPROVAL.
6. THERE SHALL BE NO CLAIM AGAINST THE VILLAGE NOR THE LANDOWNER BY THE CONTRACTOR FOR WORK PERTAINING TO MODIFICATIONS AS MAY BE REQUIRED DUE TO ANY DIFFERENCE BETWEEN ACTUAL FIELD CONDITIONS AND THOSE SHOWN BY THE DETAILS AND DIMENSIONS ON THE CONTRACT PLANS. THE CONTRACTOR WILL BE PAID AT THE UNIT BID PRICE FOR THE ACTUAL QUANTITIES OF MATERIALS USED OR FOR THE WORK PERFORMED, AS INDICATED BY THE VARIOUS ITEMS IN THE CONTRACT.
7. AT ALL TIMES, THE CONTRACTOR SHALL TAKE MEASURES TO PROVIDE POSITIVE DRAINAGE OF SURFACE RUNOFF FROM THE TRAVEL LANES AND CONTROL OF THE RUNOFF TO PREVENT EROSION, POLLUTION, SEDIMENTATION OR OTHER DISCHARGES WHICH WOULD AFFECT PROPERTIES ADJACENT TO THE WORK SITE. ALL MEASURES TAKEN TO PROVIDE POSITIVE DRAINAGE SHALL BE APPROVED BY THE ENGINEER PRIOR TO IMPLEMENTATION. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR VARIOUS ITEMS IN THE CONTRACT.
8. THE CONTRACTOR SHOULD NOTE THAT ADDITIONAL WORK MAY BE REQUIRED AS THE CONTRACT PROGRESSES WHICH IS NOT SHOWN OR NOTED ON THE PLANS. THIS WORK SHALL BE PERFORMED BY THE CONTRACTOR AS ORDERED BY THE ENGINEER AND PAYMENT SHALL BE MADE AT THE BID PRICE FOR THE APPROPRIATE ITEMS.
9. NO PAYMENT SHALL BE MADE FOR WORK CALLED FOR BY NOTES ON THE PLANS, IN THE SPECIFICATIONS, OR UNDER THE HEADING GENERAL NOTES UNLESS PAYMENT IS SPECIFICALLY INDICATED BY ITEM NUMBER. THE COST OF WORK FOR WHICH NO PAYMENT IS INDICATED SHALL BE INCLUDED IN THE UNIT PRICES BID FOR THE VARIOUS ITEMS IN THE CONTRACT.
10. WHENEVER ITEMS IN THE CONTRACT REQUIRE MATERIALS TO BE REMOVED AND DISPOSED, THE COST OF SUPPLYING A DISPOSAL AREA AND TRANSPORTATION TO THAT AREA SHALL BE INCLUDED IN THE PRICE BID FOR THOSE ITEMS.
11. THE CONTRACTOR SHALL PROVIDE ALL TEMPORARY SUPPORTS, BRACING OR OTHER DEVICES THAT MAY BE REQUIRED OR THAT MAY BE DIRECTED BY THE ENGINEER TO PROTECT THE SAFETY OF ADJACENT STRUCTURES, ROADWAYS OR THE VARIOUS ITEMS IN THE CONTRACT. NO SEPARATE PAYMENT SHALL BE MADE.
12. PAVED AREAS DISTURBED BY THE CONTRACTOR WHICH ARE NOT PART OF THE WORK TO BE PERFORMED UNDER THIS CONTRACT, SHALL BE RESTORED TO AN ACCEPTABLE CONDITION AS SPECIFIED BY AND SATISFACTORY TO THE ENGINEER, INCLUDING UN-USED ROADWAYS WITHIN THE ROW.
13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR GUARDING AND PROTECTING ALL OPEN EXCAVATIONS IN ACCORDANCE WITH THE PROVISIONS OF SECTION 107.05 "SAFETY & HEALTH REQUIREMENTS" OF THE NYSOT STANDARD SPECIFICATIONS.
14. THE CONTRACTOR SHALL KEEP ALL DRAINAGE FACILITIES, WITHIN THE CONTRACT LIMITS, CLEAN AND FULLY OPERATIONAL AT ALL TIMES (A.O.B.E.) THIS WORK SHALL BE INCLUDED UNDER VARIOUS ITEMS IN THE CONTRACT.
15. THE CONTRACTOR SHALL PROVIDE SURVEY AND STAKEOUT AS REQUIRED AND IN ACCORDANCE WITH SECTION 625 OF THE STANDARD SPECIFICATIONS. COST FOR THIS WORK SHALL BE INCLUDED UNDER ITEM 625.01-SURVEY OPERATIONS.
16. THE CONTRACTOR IS TO VISIT THE SITE BEFORE BIDDING TO BECOME FAMILIAR WITH THE PRESENT CONDITIONS AND TO JUDGE THE EXTENT AND NATURE OF THE WORK TO BE DONE UNDER THIS CONTRACT. NO EXTRA COMPENSATION WILL BE ALLOWED BECAUSE OF FAILURE TO INCLUDE IN THE BID ALL ITEMS AND MATERIALS WHICH ARE REQUIRED TO BE FURNISHED IN ACCORDANCE WITH THE CONTRACT DOCUMENTS.
17. THE CONTRACTOR SHALL BE REQUIRED TO PROTECT THEIR WORKERS AT ALL TIMES IN CONFORMANCE WITH APPLICABLE OSHA REGULATIONS.
18. DETAILS ON THE DRAWINGS LABELED AS 'NOT TO SCALE' ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY, ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONALLY AND ARE FULLY DIMENSIONED.
19. EXISTING MONUMENTS, PROPERTY CORNERS, AND SURVEY MARKERS SHALL BE PROTECTED. REPLACEMENT SHALL BE AT THE CONTRACTOR'S EXPENSE.
20. THE QUALITY OF THE UNDERGROUND UTILITY INFORMATION ON THE PLANS IS QUALITY LEVEL "C". FOUR SEPARATE QUALITY LEVELS OF SUBSURFACE UTILITY FACILITY INFORMATION ARE GENERALLY RECOGNIZED AND ARE AS FOLLOWS:

• QUALITY LEVEL A (QLA): QUALITY LEVEL A IS THE HIGHEST DEGREE OF ACCURACY. THE INFORMATION SHOWN ON THE PLANS HAS BEEN OBTAINED BY THE ACTUAL EXPOSURE (OR VERIFICATION OF PREVIOUSLY EXPOSED AND SURVEYED UTILITY FACILITIES) OF SUBSURFACE UTILITIES, USING (TYPICALLY) MINIMALLY INTRUSIVE EXCAVATION EQUIPMENT TO DETERMINE THEIR PRECISE HORIZONTAL AND VERTICAL POSITIONS, AS WELL AS THEIR OTHER UTILITY FACILITY ATTRIBUTES. (SHOWN AS QLA).

• QUALITY LEVEL B (QLB): QUALITY LEVEL B IS THE SECOND HIGHEST DEGREE OF ACCURACY. THE INFORMATION SHOWN ON THE PLANS HAS BEEN OBTAINED THROUGH THE APPLICATION OF APPROPRIATE SURFACE GEOPHYSICAL METHODS (I.E., UNDERGROUND CAMERAS, RADAR, SONAR, TONE OUTS, ETC.) TO IDENTIFY THE EXISTENCE AND APPROPRIATE HORIZONTAL POSITION OF SUBSURFACE UTILITY FACILITIES, QUALITY LEVEL B DATA ARE REPRODUCIBLE BY SURFACE GEOPHYSICS AT ANY POINT OF THEIR DEPICTION. THE INFORMATION WAS SURVEYED TO APPLICABLE TOLERANCES AND REDUCED ONTO THE PLANS. NO EXCAVATIONS WERE PERFORMED. (SHOWN AS QLB).

• QUALITY LEVEL C (QLC): QUALITY LEVEL C IS THE THIRD HIGHEST DEGREE OF ACCURACY. THE INFORMATION SHOWN ON THE PLANS HAS BEEN OBTAINED BY SURVEYING AND PLOTTING VISIBLE ABOVE-GROUND UTILITY FEATURES AND BY USING PROFESSIONAL JUDGMENT IN CORRELATING THIS INFORMATION TO QUALITY LEVEL D INFORMATION. (SHOWN AS QLC)

• QUALITY LEVEL D (QLD): QUALITY LEVEL D IS THE LOWEST DEGREE OF ACCURACY. THE INFORMATION SHOWN ON THE PLANS WAS DERIVED SOLELY FROM EXISTING NYSOT AND/OR UTILITY COMPANY RECORDS OR RECOLLECTIONS. (SHOWN AS QLD)

21. IN ADDITION TO THE CURING REQUIREMENTS OF THE NYSOT STANDARD SPECIFICATIONS, CONCRETE SIDEWALKS PLACED AFTER OCTOBER 15TH SHALL BE CURED USING WET BURLAP AND A PLASTIC COVERING. IN ADDITION, A PENETRATING SEALER SHALL BE APPLIED TO ALL SIDEWALKS PLACED AFTER SEPTEMBER 1ST PRIOR TO THE FIRST FREEZE. THE COST FOR THESE ITEMS SHALL BE INCLUDED IN THE PRICE BID FOR THE CONCRETE SIDEWALKS ITEM IN THE CONTRACT.
22. THE EST SIGNS AND ANY CORRESPONDING DIRECTIONAL ARROWS WILL BE PROVIDED BY HUDSON RIVER VALLEY GREENWAY (HRVG). THE CONTRACTOR SHALL COORDINATE THE SIGN REQUEST AND DELIVERY WITH THE VILLAGE NO LESS THAN FOUR (4) WEEKS BEFORE ANTICIPATED SIGN INSTALLATION. FAILURE TO COORDINATE EARLY WILL NOT RESULT IN ADDITIONAL PAYMENT FOR THE CONTRACTOR.

DEMOLITION AND EXCAVATION NOTES:

1. NO DEMOLITION OR EXCAVATION SHALL TAKE PLACE WITHOUT NOTIFYING "DIG SAFELY NEW YORK" AT 1-800-962-7962 OR 811.
2. ALL UTILITY LOCATIONS SHOWN ON THESE PLANS SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION.

SPILL, DISCHARGE AND CONTAMINATED SOILS NOTES:

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPORTING ANY FINDINGS OF SPILLS, LEAKS OF PETROLEUM PRODUCTS, CONTAMINATED SOILS, BURIED DRUMS OF UNKNOWN SUBSTANCES OR ANY OTHER POTENTIALLY HAZARDOUS MATERIALS TO THE NEW YORK DEPARTMENT OF ENVIRONMENTAL CONSERVATION (NYSDEC) WITHIN TWO (2) HOURS OF THE DISCOVERY. NOTIFICATION MUST BE MADE BY CALLING THE NYSDEC SPILLS HOTLINE NUMBER AT 1-800-457-7362.


SOIL EROSION AND SEDIMENT CONTROL NOTES:




1. THE CONTRACTOR WILL BE REQUIRED TO PERFORM ALL CONSTRUCTION OPERATIONS IN A MANNER SO AS TO MINIMIZE SOIL EROSION AND ENSURE SEDIMENT CONTROL. EROSION CONTROL MEASURES ARE ITEMS WHICH MINIMIZE THE EROSION OF SOIL. SEDIMENT CONTROL MEASURES ARE ITEMS WHICH KEEP SEDIMENT FROM LEAVING THE PROJECT SITE. EFFECTIVE SOIL EROSION AND SEDIMENT CONTROL CAN BE ACCOMPLISHED BY LIMITING THE AREA OF UNPROTECTED SOIL. PROTECTED IS DEFINED AS HAVING TEMPORARY OR PERMANENT SOIL EROSION CONTROL MEASURES IN PLACE. PERIMETER SEDIMENT CONTROL MEASURES ALONE ARE NOT CONSIDERED ADEQUATE PROTECTION.
2. TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED AS PER DETAILS AND SPECIFICATIONS. THE COST OF MAINTAINING AND REMOVING TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INCLUDED IN THE BID PRICE OF THE PROJECT. ALL TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSPECTED BY A QUALIFIED INSPECTOR AFTER EACH STORM EVENT OF 1/2 INCH OR MORE IN A 12 HOUR PERIOD, AT LEAST DAILY DURING PROLONGED RAINFALL. IF NO RAINFALL OCCURS, INSPECTION SHALL BE DONE ONCE EVERY SEVEN (7) CALENDAR DAYS. THE QUALIFIED INSPECTOR SHALL PREPARE AN INSPECTION REPORT SUBSEQUENT TO EACH AND EVERY INSPECTION. THE INSPECTION REPORTS SHALL BE MAINTAINED WITHIN THE CONSTRUCTION SITE LOGBOOK. WITHIN ONE (1) BUSINESS DAY OF THE COMPLETION OF AN INSPECTION, THE QUALIFIED INSPECTOR SHALL NOTIFY THE OWNER AND CONTRACTOR OF ANY CORRECTIVE ACTIONS THAT NEED TO BE TAKEN. THE CONTRACTOR SHALL BEGIN IMPLEMENTING THE CORRECTIVE ACTIONS WITHIN ONE (1) BUSINESS DAY OF THIS NOTIFICATION AND SHALL COMPLETE THE CORRECTIVE ACTIONS IN A REASONABLE TIMEFRAME.
3. ANY ADDITIONAL SOIL EROSION AND SEDIMENT CONTROL MEASURES USED TO SUPPLEMENT THE PLANS SHALL BE PREPARED IN ACCORDANCE WITH THE TECHNICAL REQUIREMENTS CONTAINED IN THE "NEW YORK STATE STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL", LATEST EDITION. ADDITIONAL SOIL EROSION AND SEDIMENT CONTROL MEASURES MAY BE REQUIRED AS PER THE PROJECT SPECIFICATIONS.
4. ALL NECESSARY PRECAUTIONS SHALL BE TAKEN TO PREVENT DIRECT OR INDIRECT CONTAMINATION OF ALL WATER WATER BODIES (INCLUDING WETLANDS) BY SILT, SEDIMENT, FUELS, SOLVENTS, LUBRICANTS, EPOXY COATINGS, CONCRETE LEACHATE, SLURRY OR OTHER POLLUTANTS ASSOCIATED WITH CONSTRUCTION PROCEDURES. DURING CONSTRUCTION, NO WET OR FRESH CONCRETE OR LEACHATE OR SLURRY SHALL BE ALLOWED TO ESCAPE DIRECTLY OR INDIRECTLY INTO ANY WATER BODIES (INCLUDING WETLANDS), NOR SHALL WASHINGS FROM CONCRETE TRUCKS, MIXERS, OR OTHER DEVICES BE ALLOWED TO ESCAPE DIRECTLY OR INDIRECTLY INTO ANY WATER BODIES (INCLUDING WETLANDS).
5. THE CONTRACTOR SHALL CONTROL FUGITIVE DUST USING WATER SPRAYS ON SOIL SURFACES AND SWEEPING PAVED AREAS AS NECESSARY.

WORK ZONE TRAFFIC CONTROL PROJECT SPECIFIC NOTES:

1. THE CONTRACTOR SHALL REFER TO THE FOLLOWING CRITERIA WHILE UTILIZING THE NYSOT STANDARD SHEETS:
 - A. PRECONSTRUCTION POSTED SPEED LIMIT = 30 MPH
 - B. TYPE OF ROADWAY: CONVENTIONAL ROAD
 - C. SETTING: URBAN
2. TIME/DATE RESTRICTIONS:
 - A. THERE SHALL BE NO TEMPORARY LANE CLOSURES ON THE FOLLOWING HOLIDAY DATES:

2023
6:00 AM MAY 25 TO 6:00 AM MAY 30
6:00 AM JUN 15 TO 6:00 AM JUN 20
6:00 AM JUN 30 TO 6:00 AM JUL 5
6:00 AM AUG 31 TO 6:00 AM SEPT 5
 - B. THERE SHALL BE NO LANE CLOSURES ON NY ROUTE 32 FROM
6:00 AM TO 9:00 AM AND FROM 3:00 PM TO 6:00 PM MONDAY THRU FRIDAY. CONTRACTOR SHALL MONITOR INTERSECTION OPERATIONS AND PREVENT EXCESSIVE QUEUING FOR CONSTRUCTION WORK THAT REQUIRES LANE CLOSURES. IF, IN THE OPINION OF THE ENGINEER/STATE, INTERSECTION OPERATION IS POOR AND SIGNIFICANT QUEUES ARE BEING OBSERVED, THE CONTRACTOR SHALL IMMEDIATELY RE-OPEN THE LANE, AOB.
 - C. THERE SHALL BE NO WORK OPERATIONS ALLOWED BEFORE DAWN OR AFTER SUNSET WITHOUT APPROVAL OF THE VILLAGE/ENGINEER/STATE AND AN APPROVED LIGHTING PLAN. THE LIGHTING PLAN SHALL BE APPROVED PRIOR TO THE START OF WORK BY THE ENGINEER. PAYMENT FOR NIGHT WORK SHALL BE INCLUDED UNDER THE LUMP SUM PRICE BID FOR ITEM 619.24.

AFFIX SEAL: ON: 04/07/2023	ALTERED BY: ON:
	

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	WALLKILL VALLEY RAIL TRAIL CONNECTION	PIN	BRIDGES	CULVERTS	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED	CONTRACT NUMBER
					GENERAL NOTES	DRAWING NO. GNN-01 SHEET NO. 6
	VILLAGE OF NEW PALTZ					
	COUNTY: ULSTER REGION: 8					
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						
		1801 6th Avenue, Suite 204 Troy, NY 12180 518-874-4211				VILLAGE OF NEW PALTZ

FILE NAME = 2021-071.C-DET.DWG
DATE/TIME = 4/7/2023 5:04 PM
USER = MIKEREPSCH

DESIGN SUPERVISOR K. DI COCCO

JOB MANAGER K. DI COCCO

DESIGN D. JLELATY


CHECK K. DI COCCO

DRAFTING D. JLELATY

CHECK K. DI COCCO




PROJECT MANAGER K. DI COCCO

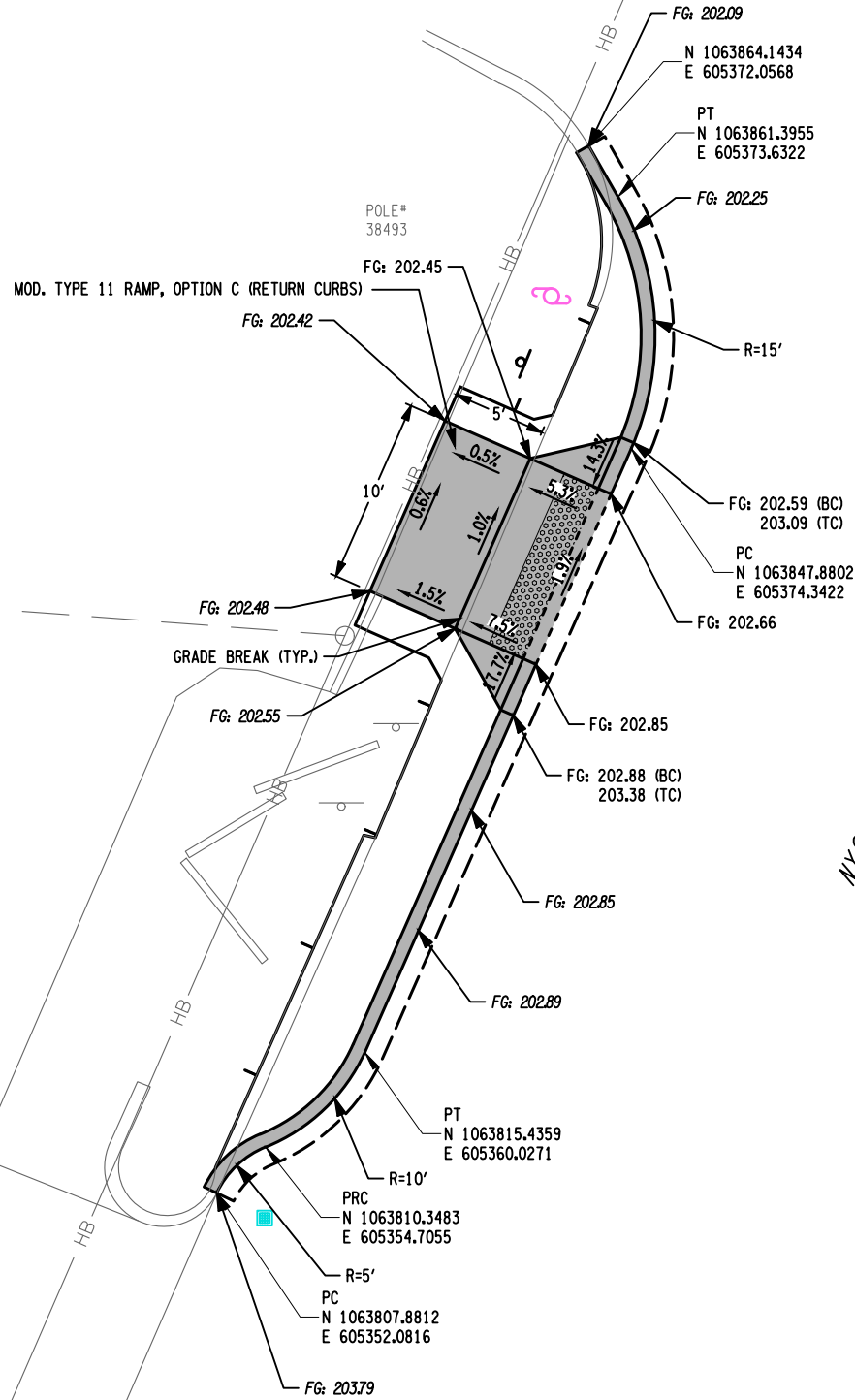
AFFIX SEAL:
ON: 04/07/2023



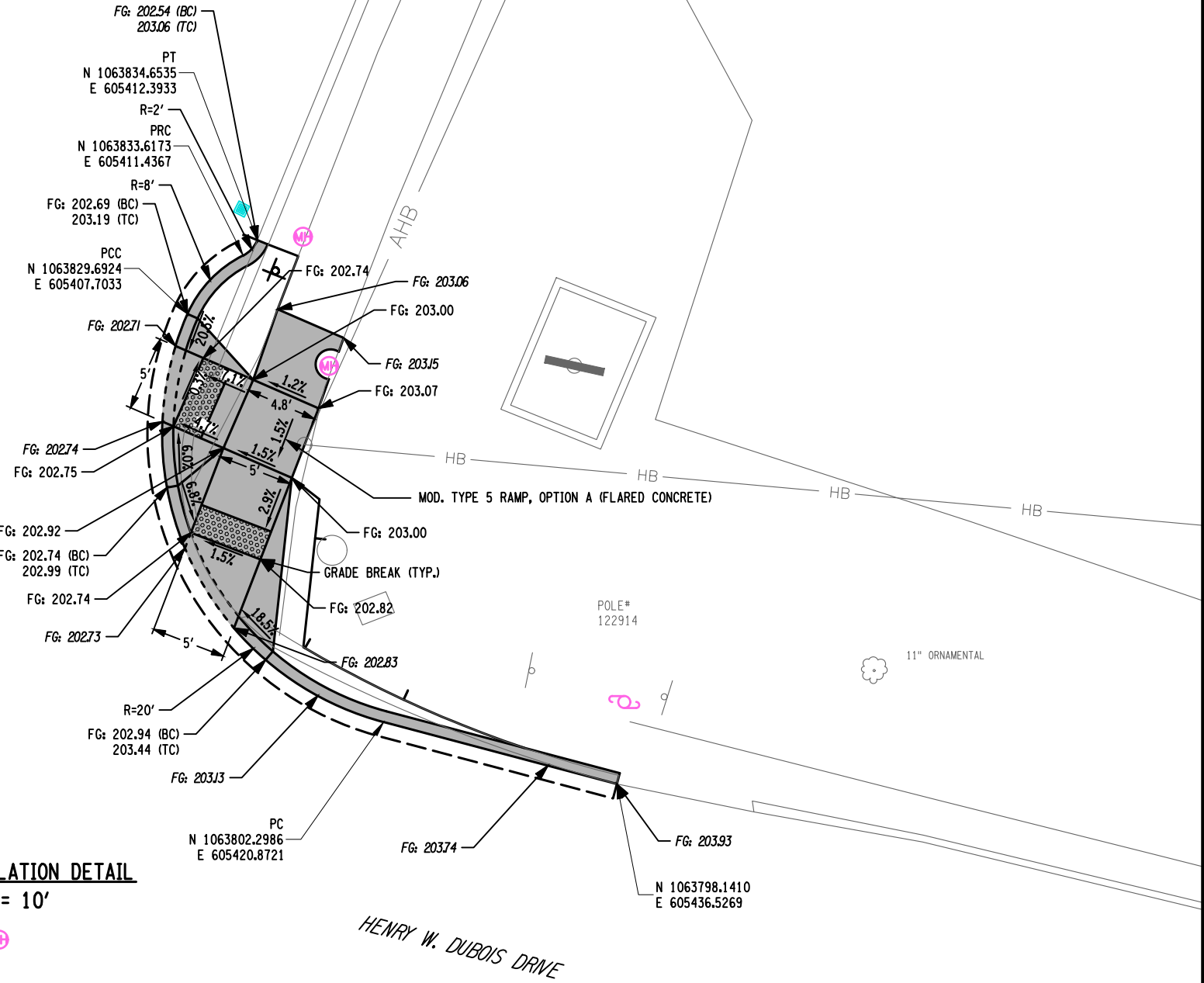
ALTERED BY:
ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	WALLKILL VALLEY RAIL TRAIL CONNECTION	PIN	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER
					MISCELLANEOUS DETAILS	
	VILLAGE OF NEW PALTZ					DRAWING NO. MSD-01 SHEET NO. 7
	COUNTY: ULSTER REGION: 8					

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.	 www.altago.com	1801 6th Avenue, Suite 204 Troy, NY 12180 518-874-4211	 Engineering + Landscape Architecture, PLLC		VILLAGE OF NEW PALTZ
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CURB RAMP INSTALLATION DETAIL
SCALE: 1" = 10'



NOTE: THE EST SIGNS AND ANY CORRESPONDING DIRECTIONAL ARROWS WILL BE PROVIDED BY HUDSON RIVER VALLEY GREENWAY (HRVG). THE CONTRACTOR SHALL COORDINATE THE SIGN REQUEST AND DELIVERY WITH THE VILLAGE NO LESS THAN FOUR (4) WEEKS BEFORE ANTICIPATED SIGN INSTALLATION. FAILURE TO COORDINATE EARLY WILL NOT RESULT IN ADDITIONAL PAYMENT FOR THE CONTRACTOR.

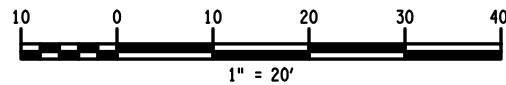
LEGEND



PROPOSED CONCRETE




PROPOSED GROUNDCOVER

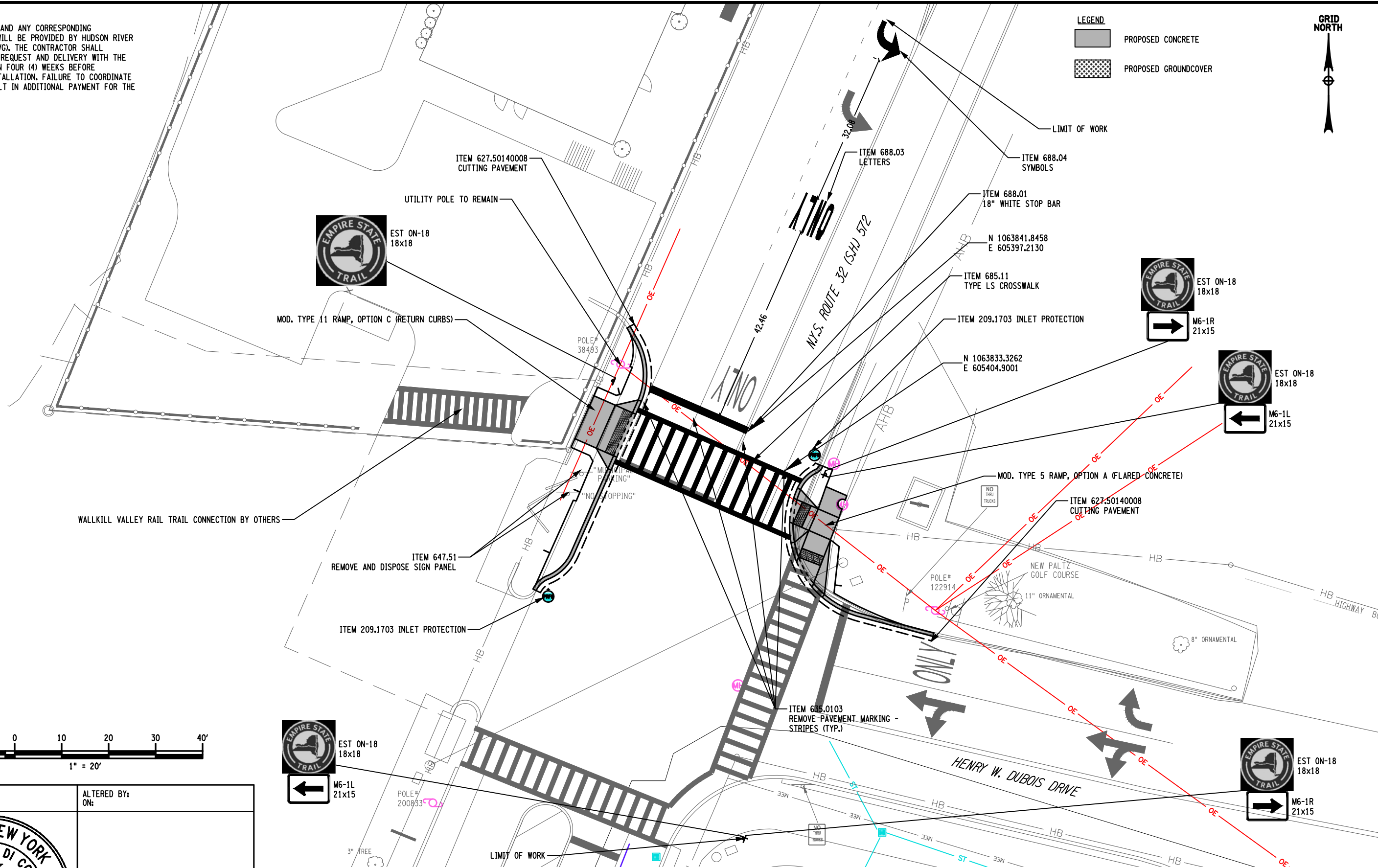
GRID NORTH



AFFIX SEAL:
ON: 04/07/2023

ALTERED BY:
ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	WALLKILL VALLEY RAIL TRAIL CONNECTION		PIN	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
						GENERAL PLAN SHEET	DRAWING NO. GNP-01 SHEET NO. 8	
	VILLAGE OF NEW PALTZ							
	COUNTY: ULSTER	REGION: 8						
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						 1801 6th Avenue, Suite 204 Troy, NY 12180 518-874-4211 www.altago.com	 Engineering + Landscape Architecture, LLC	 VILLAGE OF NEW PALTZ




GENERAL NOTES




1. ALL TRAFFIC SIGNAL EQUIPMENT SHALL BE INSTALLED IN ACCORDANCE WITH THE APPLICABLE NEW YORK STATE STANDARD SHEETS EXCEPT AS MODIFIED BELOW OR IN THE CONTRACT PLANS.
2. ALL NATIONAL ELECTRICAL MANUFACTURERS ASSOCIATION (NEMA) SPECIFICATION EQUIPMENT SHALL MEET THE REQUIREMENTS OF THE CURRENT NEMA SPECIFICATION FOR TRAFFIC CONTROL EQUIPMENT.
3. THE CONTRACTOR SHALL HAVE ALL CONVENTIONAL, NEMA OR INTERSECTION FLASHER EQUIPMENT INSPECTED BY NEW YORK STATE SIGNAL MAINTENANCE FORCES BEFORE THE SIGNAL IS ENERGIZED. THE CONTRACTOR SHALL MAINTAIN EACH SIGNAL IN CONTINUOUS OPERATION AS SPECIFIED BY THE TABLE OF OPERATIONS FOR 30 DAYS BEFORE SEEKING FINAL ACCEPTANCE
4. UPON COMPLETION OF WORK AT AN INTERSECTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER THAT THE SIGNAL IS READY FOR INSPECTION BY NYSDOT TRAFFIC SIGNAL MAINTENANCE PERSONNEL AT LEAST TWO WEEKS PRIOR TO DESIRED INSPECTION DATE. AT THE TIME OF INSPECTION, THE CONTRACTOR SHALL UNBAG THE NEW SIGNAL HEADS AND PROVIDE ACCESS TO ALL PULLBOXES FOR INSPECTION PURPOSES. THE CONTRACTOR WILL BE NOTIFIED OF ANY DEFECTS FOUND DURING THE INSPECTION, AND SHALL MAKE THE NECESSARY CORRECTIONS FOR THE SIGNAL SYSTEM TO BE ACCEPTED. THE CONTRACTOR SHALL PROVIDE SUFFICIENT PERSONNEL AND TRAFFIC CONTROL DEVICES TO SAFELY MAINTAIN TRAFFIC THROUGH THE INTERSECTION WHILE THE INSPECTION IS BEING PERFORMED. THE CONTRACTOR SHALL REBAG THE SIGNAL HEADS SHOULD THE SIGNAL SYSTEM FAIL.
5. THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN AN EXISTING TRAFFIC SIGNAL INCLUDES MAINTAINING VEHICLE DETECTORS ON A VEHICLE ACTUATED INSTALLATION. IF A VEHICLE DETECTOR BECOMES INOPERATIVE, THE CONTRACTOR SHALL REPAIR OR REPLACE IT. IF A NEW COMPATIBLE DETECTOR IS CALLED FOR IN THE PLANS, THE CONTRACTOR MAY, WITH THE PERMISSION OF THE EIC, CONNECT THE NEW DETECTOR INTO THE EXISTING SYSTEM.
6. UNLESS OTHERWISE NOTED, THE STATE OF NEW YORK WILL HAVE MAINTENANCE JURISDICTION OVER ALL SIGNALS IN THIS CONTRACT UPON COMPLETION OF THEIR INSTALLATION AND OFFICIAL ACCEPTANCE.
7. ALL SIGNAL IMPROVEMENTS SHOWN ON THE PLANS, INCLUDING LANE DESIGNATION SIGNS, TURNING LANES, CHANNELIZATION, PAVEMENT MARKINGS, ETC., SHALL BE IN PLACE BEFORE THE NEW SIGNAL SYSTEM IS PLACED INTO OPERATION.
8. THE CONTRACTOR SHALL ESTABLISH THE LOCATIONS OF THE UNDERGROUND UTILITIES AND SUPPORT AND PROTECT ALL LINES ENCOUNTERED IN THE TRENCHING AND EXCAVATION OPERATIONS.
9. IF IT IS DISCOVERED THAT MINIMUM CLEARANCES FROM PRIMARY AND/OR SECONDARY POWER CONDUCTORS REQUIRED BY THE NATIONAL ELECTRICAL SAFETY CODE (ANSI STANDARD C2-2017) AND LOCAL UTILITY CODES CANNOT BE ACHIEVED, THE UTILITY COMPANY OWNING SUCH CONDUCTORS SHALL BE NOTIFIED IN WRITING BY THE CONTRACTOR. NO FURTHER WORK SHALL BE DONE UNTIL SAID POWER LINES HAVE BEEN RELOCATED TO PROVIDE THE PROPER CLEARANCES.
10. IF IT IS NECESSARY TO RELOCATE A SIGNAL POLE MORE THAN 5 FEET FROM THE LOCATION SHOWN ON THE CONTRACT PLANS, THE CONTRACTOR SHALL NOTIFY THE EIC AND SEEK AN ALTERNATE LOCATION FROM THE DESIGNERS.
11. ALL POLE BASES SHALL HAVE ON UNUSED CONDUIT IN THE BASE; THIS CONDUIT SHALL BE RUN TO THE NEAREST PULLBOX.
12. WHERE ONE CONDUIT IS TOO SMALL TO PERMIT THE PASSAGE OF THE REQUIRED SIGNAL WIRING, TWO OR MORE CONDUITS SHALL BE USED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE NUMBER OF CONDUITS NEEDED.
13. INDIVIDUAL LENGTHS OF LENGTHS OF GALVANIZED STEEL CONDUIT SHALL BE CONNECTED TOGETHER WITH THREADED GALVANIZED STEEL COUPLINGS; SLIP-FIT TYPE COUPLINGS MAY ONLY BE USED BETWEEN INDIVIDUAL BRIDGE SPANS OR BETWEEN A BRIDGE SPAN AND AN ABUTMENT TO ALLOW FOR THERMAL EXPANSION AND CONTRACTION OF THE INDIVIDUAL PARTS OF THE BRIDGE.
14. LEAD-INS FROM ALL TRAFFIC SIGNAL DETECTORS SHALL BE TAGGED OR DIAGRAMMED IN THE CONTROLLER CABINET TO IDENTIFY THE DETECTORS THAT THEY SERVE,
15. IF UNANTICIPATED SOUND ROCK IS ENCOUNTERED WITHIN ONE DIAMETER OF THE BOTTOM OF THE SHAFT EXCAVATION (AS DETERMINED FROM THE "MINIMUM EMBEDMENT" LENGTH IN THE FOUNDATION TABLE ON STANDARD SHEET 680-01), USE THE FULL SHAFT LENGTH. IF UNANTICIPATED SOUND ROCK IS ENCOUNTERED AT HIGHER ELEVATIONS, THE TOTAL SHAFT LENGTH MAY BE DECREASED SUCH THAT THE SHAFT PENETRATES A MINIMUM OF 1 DIAMETER INTO SOUND ROCK. HOWEVER, THE TOTAL SHAFT EMBEDMENT SHALL NOT BE LESS THAN 1 DIAMETER PLUS 2 FEET. IF THE RESULTING DEPTH IS LESS THAN THE LENGTH OF THE ANCHOR BOLTS, THE CONTRACTOR SHALL DRILL INTO THE ROCK TO THE ANCHORING SYSTEM. ALL CHANGES TO SHAFT LENGTHS MUST BE APPROVED AND AS ORDERED BY THE ENGINEER.
16. FOR NEW SIGNAL POLE INSTALLATIONS, THE CONCRETE POLE FOUNDATION SHALL CURE FOR A MINIMUM OF 7 DAYS BEFORE THE SIGNAL POLE IS ERECTED, AND FOR A MINIMUM OF 14 DAYS BEFORE THE SPAN WIRE AND SIGNAL HEADS ARE INSTALLED. AS SOON AS THE POLE BASES ARE CAST, THE ENGINEER-IN-CHARGE SHALL CONTACT REGIONAL DESIGN PERSONNEL, SO THAT THEY MAY PROVIDE THE CONTRACTOR WITH THE SPAN WIRE ATTACHMENT HEIGHTS.
17. ALL ANCHOR BASE TRAFFIC SIGNAL SPAN WIRE, MAST ARM, AND PEDESTRIAN SIGNAL SIGNAL HEAD POLES SHALL BE EQUIPPED WITH ANCHOR BOLT COVERS.
18. ALL POLES SHALL BE EQUIPPED WITH A GROUNDING TERMINAL LOCATED ON THE OPPOSITE SIDE OF THE POLE FROM THE HANDHOLE. GROUNDING TERMINALS LOCATED ADJACENT TO THE HANDHOLE WILL NOT BE ALLOWED.

19. ALL HOLES FOR L.B. CONDULETS, THIMBLEBOLTS, AND POLE-MOUNTED SIGNAL AND/OR PEDESTRIAN HEADS SHALL BE DRILLED. THE USE OF CUTTING TORCHES IS NOT PERMITTED.
20. WHERE SIGNAL CABLE IS INSTALLED ON SPAN WIRE OR MESSENGER CABLE, IT SHALL BE SUPPORTED AT INTERVALS NOT GREATER THAN 15 INCHES BY COPPERWELD (COPPER COVERED STEEL) CABLE RINGS APPROVED BY THE ENGINEER. STEEL OR PLASTIC CABLE BANDS, OR TAPE SHALL NOT BE USED.
21. ALL NEW AND EXISTING SIGNAL SYSTEMS TO BE UPDATED WITH THE NEW SIGNAL CONTROLLERS SHALL BE EQUIPPED WITH AN ELECTRICAL DISCONNECT/GENERATOR TRANSFER SWITCH (ITEM 680.94998008) WHICH SHALL BE INSTALLED BETWEEN THE METER SOCKET AND THE CONTROLLER. THE SERVICE CABLE SHALL RUN FROM THE CHASE NIPPLE IN THE BACK OF THE DISCONNECT BOX INTO THE SIGNAL POLE AND THEN THROUGH THE LARGE L.B. CONDULET INTO THE CONTROLLER CABINET.
22. THE LOCATION AND ORIENTATION OF THE POLE MOUNTED CONTROLLER CABINET SHALL BE BASED ON THE FOLLOWING:
A) PROVIDING SAFE ACCESS TO THE CONTROLLER FROM WITHIN THE RIGHT OF WAY.
B) PROVIDING PROTECTION TO THE CONTROLLER FROM SNOWPLOWS AND ERRANT VEHICLES.
C) PREVENTING THE CONTROLLER FROM OVERHANGING THE SIDEWALK OR PRESENTING A HAZARD TO PEDESTRIANS.
D) PROVIDING VISIBILITY OF THE SIGNAL HEADS DURING MAINTENANCE OPERATIONS.
THE CABINET SHALL BE INSTALLED SO THAT THE BOTTOM OF THE CABINET IS 18 INCHES ABOVE THE STANDING PAD.
23. EACH PHASE SHALL HAVE ITS OWN GROUND WIRE.
24. A CARD SHALL BE ATTACHED TO THE INSIDE DOOR OF EACH CONTROLLER CABINET AND LIST ALL THE PARTS BY SERIAL NUMBER, THE DATE OF INSTALLATION, AND THE DATE OF OFFICIAL OPERATION. WHERE STATE MICROCOMPUTERS ARE INSTALLED, THIS WILL BE DONE BY STATE SIGNAL MAINTENANCE PERSONNEL.
25. THE CONTRACTOR SHALL INSTALL BALANCE ADJUSTERS AND SWIVEL BALANCERS AT EACH SIGNAL HEAD ASSEMBLY.
26. ON SIGNAL HEAD ASSEMBLIES, ALL FEMALE THREADED CONNECTING HARDWARE SHALL HAVE TWO SET SCREWS AT 90° AND ALL THREADED PIPE NIPPLES SHALL HAVE TAPERED THREADS
27. THE BOTTOMS OF ALL TRAFFIC SIGNAL HEADS SHALL BE LOCATED AT LEAST 15 FEET 6 INCHES ABOVE THE PAVEMENT SURFACE.
28. ALL SIGNAL CONTROL CABLE CONNECTIONS TO THE SIGNAL HEADS SHALL BE MADE DIRECTLY TO THE THE TERMINAL BLOCK(S) INSIDE OF EACH SIGNAL HEAD ASSEMBLY; PIGTAILS AND EXPOSED SPLICES SHALL NOT BE PERMITTED. WHERE TWO SIGNAL HEADS ARE TO BE WIRED FROM THE SAME CABLE, THE SIGNALS SHALL NOT BE CONNECTED IN PARALLEL ("DAISY CHAINED") WITH ONE LENGTH OF WIRE RUNNING FROM THE CONTROLLER TO THE FIRST SIGNAL'S WEATHERHEAD AND A SECOND LENGTH LEAVING THE WEATHERHEAD AND RUNNING TO THE OTHER SIGNAL'S WEATHERHEAD.
29. ONCE TRAFFIC SIGNAL HEAD ASSEMBLIES HAVE BEEN INSTALLED AND ADJUSTED IN THE FIELD TO THE SATISFACTION OF STATE TRAFFIC MAINTENANCE PERSONNEL, THE CONTRACTOR SHALL APPLY A BEAD OF SILICONE SEALANT AROUND THE SERRATED LOCK RING AT ALL LOCATIONS WHERE VERTICAL PIPE NIPPLES ARE ATTACHED TO THE TOP SURFACE OF BOTH TRAFFIC SIGNAL SECTIONS AND PIPE CROSS BODIES TO PREVENT WATER INFILTRATION INTO THE SIGNAL ASSEMBLY.
30. UNLESS OTHERWISE NOTED, ALL TRAFFIC SIGNAL HEADS, STEEL ANCHOR BASE SIGNAL POLES, MAGNETIC DETECTOR PROBES, MICROWAVE DETECTORS, PULLBOX FRAMES AND COVERS, AND STATE MICROCOMPUTER CABINETS WHICH ARE TO BE REMOVED SHALL BECOME THE PROPERTY OF THE STATE OF NEW YORK. THE CONTRACTOR SHALL DELIVER THIS EQUIPMENT TO THE TRAFFIC SIGNAL SHOP WITHIN TWO WEEKS OF SIGNAL ACCEPTANCE BY STATE SIGNAL SIGNAL MAINTENANCE PERSONNEL. ALL OTHER MATERIALS AND EQUIPMENT SHALL BE DISPOSED OF AND THE SURROUNDING AREA SHALL BE RESTORED BY THE CONTRACTOR WITHIN FOUR WEEKS OF ACCEPTANCE OF THE SIGNAL.
31. PEDESTRIAN PUSHBUTTONS SHALL BE LOCATED NEAR CURB RAMPS AND POSITIONED SUCH THAT A PERSON IN A WHEEL CHAIR CAN ACTIVATE THE SIGNAL WITHOUT HAVING TO STOP ON THE CURB RAMP. ALL PEDESTRIAN PUSHBUTTON ASSEMBLIES SHALL BE ADA COMPLIANT.
32. ALL NEW OR MODIFIED SIDEWALK RAMPS BE EQUIPMENT WITH CAST IRON EMBEDDED WITH CAST IRON DETECTABLE WARNING FIELDS. SEE STANDARD SHEETS 608-01 FOR DETAILS.
33. CONTACT THE REGION 8 SIGNAL SHOP AT XXX-XXX-XXXX FOR STATE-SUPPLIED EQUIPMENT AND SIGNAL INSPECTION.

AFFIX SEAL:
ON: 04/07/2023

ALTERED BY:
ON:



AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	WALLKILL VALLEY RAIL TRAIL CONNECTION	PIN	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER		
					TRAFFIC SIGNAL NOTES	DRAWING NO. TSN-01 SHEET NO. 9		
	VILLAGE OF NEW PALTZ							
	COUNTY: ULSTER REGION: 8							
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ESTIMATE OF QUANTITIES			
ITEM	DESCRIPTION	UNIT	QUANTITY
206.03	CONDUIT EXCAVATION AND BACKFILL INCLUDING SURFACE RESTORATION	LF	100
680.5001	POLE EXCAVATION AND CONCRETE FOUNDATION	CY	2.2
680.510501	PULLBOX, RECTANGULAR 26" X 18", RC	EA	1
680.520506	CONDUIT, RIGID PLASTIC, CLASS 1, 2"	LF	20
680.520508	CONDUIT, RIGID PLASTIC, CLASS 1, 3"	LF	80
680.670801	PEDESTRIAN SIGNAL POLE--POST TOP MOUNT (8-FOOT)	EA	1
680.730214	SIGNAL CABLE, 2 CONDUCTORS, 14 AWG	LF	300
680.730514	SIGNAL CABLE, 5 CONDUCTORS, 14 AWG	LF	285
680.81310109	ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITH POLE	EA	1
680.81310209	ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITHOUT POLE	EA	1
680.813105	PEDESTRIAN SIGNAL MODULE - 12 INCH BI-MODAL, HAND/MAN SYMBOLS LED	EA	2
680.8141	PEDESTRIAN SIGNAL BRACKET SIDE MOUNT ASSEMBLY	EA	1
680.8142	PEDESTRIAN SIGNAL BRACKET TOP MOUNT ASSEMBLY	EA	1
680.8207	OVERHEAD SIGN ASSEMBLY	EA	1


TABLE OF OPERATIONS												
	1	2	3	4	5	6	7	8	9	PED A	PED B	PED C
Ø1	R	R	R	R	R	R	R	R	R	DW	DW	DW
Ø2	R	G	R	G	R	R	R	R	R	DW	DW*	DW
Ø6	R	R	R	R	R	R	G	R	G	DW	DW	DW
Ø1+Ø6	R	R	R	R	R	G	G	R	G	DW	DW	DW
Ø2+Ø6	R	G	R	G	R	G	G	R	G	DW	DW	DW
Ø4	R	R	R	R	G	R	R	G	R	DW	DW	DW*
Ø8	G	R	G	R	R	R	R	R	R	DW*	DW	DW
Ø4+Ø8	G	R	G	R	G	R	R	G	R	DW	DW	DW
FLASH OPERATION	FR	FY	FR	FY	FR	FY	FY	FR	FY	DARK	DARK	DARK

NOTE:*

1. PEDESTRIAN SIGNAL TO BE DEMAND ACTUATED ONLY UPON ACTUATION OF A PEDESTRIAN PUSH BUTTON. THE INDICATION SHALL DISPLAY A STEADY "MAN" FOLLOWED BY A FLASHING "HAND" AND COUNTDOWN TIMER AND THEN A STEADY "HAND".

AFFIX SEAL:
ON: 04/07/2023

ALTERED BY:
ON:



AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

WALLKILL VALLEY RAIL TRAIL CONNECTION

PIN

BRIDGES

CULVERTS

ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED

CONTRACT NUMBER

VILLAGE OF NEW PALTZ

REGION: 8

TRAFFIC SIGNAL PLAN

DRAWING NO. TSP-01

SHEET NO. 10

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

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Troy, NY 12180
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H+Z
Engineering & Landscape
Architecture, PLLC



VILLAGE OF
NEW PALTZ

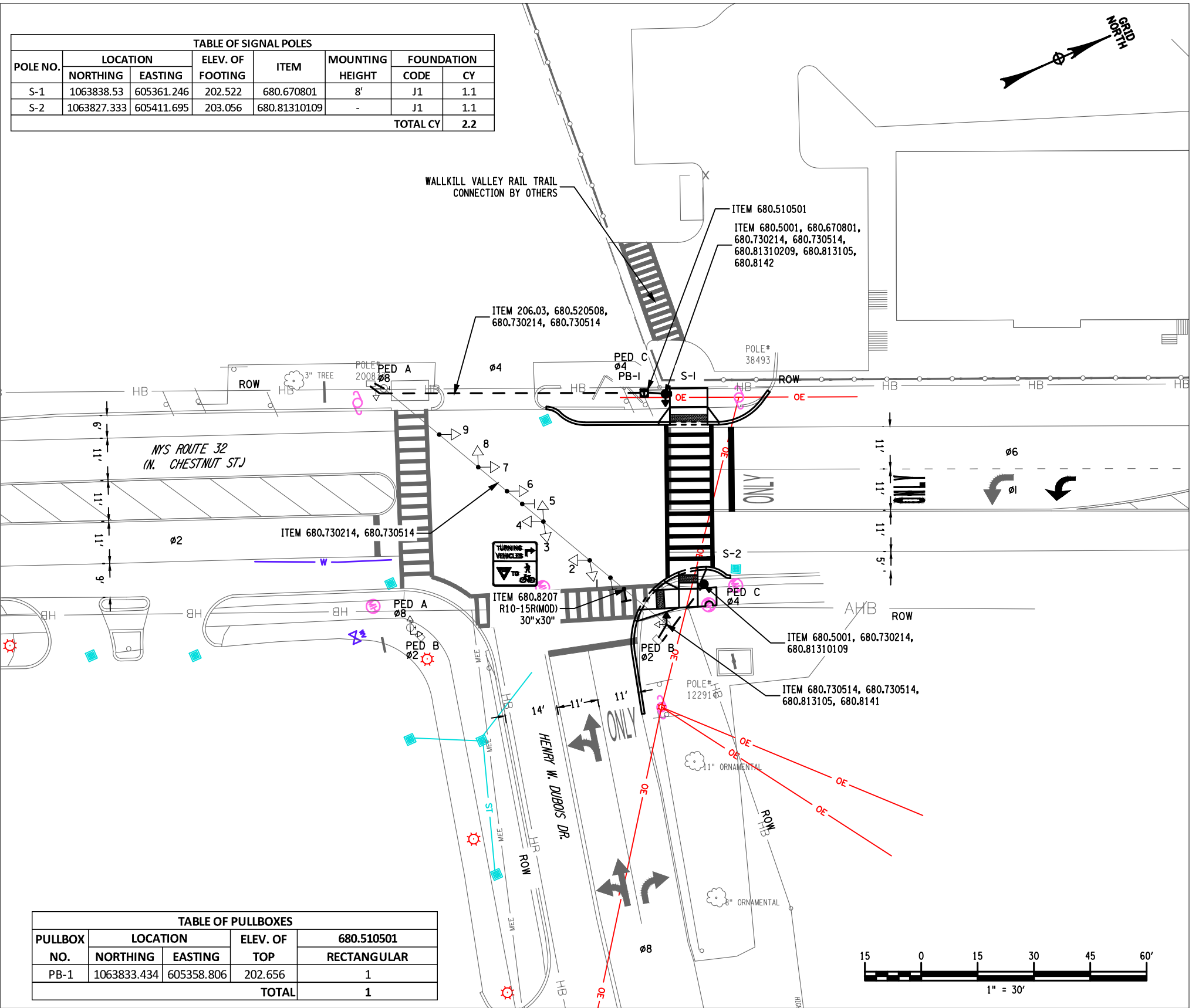


TABLE OF PULLBOXES				
PULLBOX	LOCATION		ELEV. OF	680.510501
NO.	NORTHING	EASTING	TOP	RECTANGULAR
PB-1	1063833.434	605358.806	202.656	1
TOTAL				1

FILE NAME = 619-010.dgn
DATE/TIME = 22-DEC-2022 08:50
USER = rfoote

GENERAL NOTES

1. THE TYPICAL DETAILS DEPICTED ON THE STANDARD SHEETS AND IN THE MUTCD, REFLECT THE MINIMUM REQUIREMENTS.
2. PROPOSED REVISIONS TO THE TRAFFIC CONTROL PLAN SHALL BE PROVIDED, IN WRITING, TO THE DOT ENGINEER FOR REVIEW AND APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE FIVE (5) WORK DAYS PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH PROPOSED REVISIONS, EXCEPT FOR CHANGES THAT ALTER THE SCOPE OF THE TRAFFIC CONTROL PLAN. SUCH CHANGES IN SCOPE MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE THIRTY (30) WORK DAYS PRIOR TO IMPLEMENTATION OF SUCH REVISIONS.
3. THE NAMES, ADDRESSES, AND TELEPHONE NUMBERS OF STAFF WHO ARE AUTHORIZED TO SECURE LABOR, MATERIALS, AND EQUIPMENT FOR EMERGENCY REPAIRS OUTSIDE NORMAL WORKING HOURS SHALL BE PROVIDED, IN WRITING, TO THE NYSDOT ENGINEER. THE ENGINEER WILL PROVIDE THE SUBMITTED INFORMATION TO REGIONAL MANAGEMENT, THE NEW YORK STATE POLICE, THE RESIDENT ENGINEER, AND THE LOCAL POLICE.
4. STANDARD SHEET 619-503 MAY BE USED FOR AN OFFSITE DETOUR SETUP FOR BOTH LONG TERM AND SHORT TERM WORK DURATIONS.
5. REGIONAL HIGH-VOLUME RESTRICTIONS SHALL BE FOLLOWED. CONSULT WITH DOT ENGINEER IF EXCEPTION NEEDED.
6. PLAN AHEAD TO AVOID CONFLICTING WORK ZONES. CHECK FOR CONSTRUCTION PROJECTS, CLOSURES, & RESTRICTIONS AT WWW.511NY.ORG, WWW.DOT.NY.GOV/PROJECTS, AND WITH NYSDOT ENGINEER.
7. WORK ZONE INCIDENTS SHALL BE DOCUMENTED AND REPORTED USING EITHER THE DEPARTMENT'S WORK ZONE INCIDENT FORM, OR THE CONSTRUCTION INCIDENT REPORTING SYSTEM, AS APPROPRIATE.
8. CONSIDER CLOSURE WIDTH AND THE ABILITY TO ACCOMMODATE WIDE LOAD VEHICLES BEFORE ESTABLISHING WORK ZONES.
9. IF THE WORK ZONE AFFECTS AN EXISTING ACCESSIBLE AND DETECTABLE PEDESTRIAN FACILITY, ACCESSIBILITY AND DETECTABILITY SHALL BE PROVIDED ALONG THE ALTERNATE ROUTE.

ACTIVITY AREA

1. A 500' MINIMUM LONGITUDINAL DISTANCE SHALL BE MAINTAINED BETWEEN CONSTRUCTION OPERATIONS ON ALTERNATE SIDES OF THE ROADWAY, UNLESS OTHERWISE APPROVED BY THE ENGINEER.
2. WHEN TWO OR MORE AREAS ARE ADJACENT, OVERLAP, OR ARE IN CLOSE PROXIMITY, THE CONTRACTOR SHALL ENSURE THERE ARE NO CONFLICTING SIGNS AND THAT LANE CONTINUITY IS MAINTAINED THROUGHOUT ALL WORK AREAS.

SIGNS

1. THE LOCATIONS OF THE SIGNS SHOWN ON THE WORK ZONE TRAFFIC CONTROL PLANS AND DETAILS MAY BE ADJUSTED BASED ON SIGHT DISTANCE AND OTHER CONSIDERATIONS. THE FINAL LOCATIONS OF SIGNS ARE SUBJECT TO APPROVAL OF THE ENGINEER.
2. FOR LONG TERM WORK DURATIONS, ANY EXISTING SIGNS, INCLUDING OVERHEAD SIGNS, WHICH CONFLICT WITH THE TEMPORARY TRAFFIC CONTROL SIGN LAYOUT SHALL BE COVERED, REMOVED, STORED OR RESET, AS APPROVED BY THE ENGINEER. ALL APPROPRIATE EXISTING SIGNS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AND/OR LOCATION UNLESS OTHERWISE REPLACED IN THIS CONTRACT.
3. SIGNS AT OR NEAR INTERSECTIONS SHALL BE PLACED SO THAT THEY DO NOT OBSTRUCT A MOTORIST'S LINE OF SIGHT.
4. SIGNS MOUNTED ON THE MEDIAN OF DIVIDED HIGHWAYS WHERE MEDIAN BARRIER IS IN PLACE MAY BE MOUNTED ON THE BARRIER WITH A SADDLE TYPE BRACKET OR OMITTED WITH THE APPROVAL OF THE DOT ENGINEER. LAYING THE SIGN DOWN IN A HORIZONTAL POSITION IS NOT PERMITTED.
5. THE DIMENSIONS OF WORK ZONE TRAFFIC CONTROL SIGNS ARE DESCRIBED IN THE MUTCD. ANY CHANGES TO THE DIMENSIONS SHALL BE APPROVED BY THE REGIONAL DIRECTOR OR BY HIS/HER DESIGNEE.
6. NYR9-12 SHALL BE USED IN PLACE OF NYR9-11 WHEN A REDUCED REGULATORY SPEED LIMIT SIGN IS AUTHORIZED.
7. RIGID AND FLEXIBLE "ROLL-UP" SIGNS MAY BE USED FOR MOBILE, SHORT DURATION AND SHORT-TERM STATIONARY WORK. RIGID SIGNS MUST BE MOUNTED AT LEAST 5 FEET ABOVE GRADE (7 FEET WHERE THERE ARE PEDESTRIANS OR PARKED CARS). FLEXIBLE SIGNS SHALL BE MOUNTED AT LEAST ONE FOOT ABOVE GRADE. MESH SIGNS SHALL NOT BE USED. USE RETRO REFLECTORIZED RIGID SIGNS FOR NIGHTTIME WORK.

CHANNELIZING DEVICES

1. WHERE POSSIBLE ALL CHANNELIZING AND GUIDING DEVICES ARE TO BE PLACED SO AS TO PROVIDE A MINIMUM 2' LATERAL CLEARANCE TO THE TRAVELED WAY.

PUBLIC ACCESS

1. PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE INACCESSIBLE SHALL BE NOTIFIED AT LEAST 24 HOURS PRIOR TO RESTRICTING USE OF THE DRIVEWAY. FOR MULTIPLE ACCESS PROPERTIES, AT LEAST ONE DRIVEWAY SHALL BE OPEN AT ALL TIMES. ACCESS SHALL BE RESTORED TO ALL DRIVEWAYS AS SOON AS POSSIBLE.
2. SUITABLE RAMPS SHALL BE INSTALLED TO MAINTAIN SMOOTH TRANSITIONS FROM RESIDENTIAL AND COMMERCIAL DRIVEWAYS TO AND FROM THE WORK AREA.

LANE CLOSURES

1. LANE CLOSURES SHALL BE LOCATED TO PROVIDE OPTIMUM VISIBILITY, I.E. BEFORE CURVES AND CRESTS, TO THE EXTENT CONDITIONS PERMIT.
2. THE ENGINEER MAY REQUIRE THAT ALL LANES BE RE-OPENED AT ANY TIME IF THE ROUTE IS NEEDED FOR EMERGENCY PURPOSES. THIS COULD INCLUDE INCIDENTS AT LOCATIONS OUTSIDE THE CONTRACT LIMITS.
3. EACH ARROW PANEL SHALL BE VISIBLE 1500 FEET IN ADVANCE FROM ANY POINT WITHIN THE ROADWAY.

LANE WIDTHS

1. UNLESS AUTHORIZED BY THE ENGINEER, THE MINIMUM LANE WIDTHS FOR WORK ZONE TRAVEL LANES SHALL BE AS FOLLOWS: FREEWAYS AND/OR EXPRESSWAYS IS 11'. THE MINIMUM LANE WIDTH FOR ALL OTHER TYPES OF ROADWAYS IS 10'.
2. A WRITTEN NOTE SHALL BE PROVIDED TO THE ENGINEER, A MINIMUM OF 21 CALENDAR DAYS IN ADVANCE OF PERFORMING ANY WORK THAT RESULTS IN THE REDUCED WIDTH OF AN EXISTING ROADWAY, SO THAT THE ENGINEER MAY NOTIFY THE REGIONAL PERMIT ENGINEER IN A TIMELY MANNER.
3. IF THE WORK ZONE AFFECTS PEDESTRIANS, A MINIMUM PEDESTRIAN PATHWAY WIDTH OF 5 FEET SHALL BE MAINTAINED UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.
4. TEMPORARY BICYCLE ACCOMMODATIONS SHALL NOT BE LESS THAN WHAT CURRENTLY EXISTS UNLESS AUTHORIZED BY THE ENGINEER.

PROTECTIVE VEHICLES

1. PROTECTIVE VEHICLES ARE DIVIDED INTO 2 CATEGORIES BASED ON THE GROSS VEHICLE WEIGHT (GVW):
 - PROTECTIVE VEHICLE LIGHT (PVL) SHALL HAVE A MINIMUM GVW OF 9,500 LBS. OR GREATER.
 - PROTECTIVE VEHICLE HEAVY (PVH) SHALL HAVE A MINIMUM GVW OF 22,000 LBS. OR GREATER.
2. IF THE PROTECTIVE VEHICLE ENCROACHES INTO THE TRAVEL LANE, OR IF IT REMAINS ENTIRELY ON THE SHOULDER OF ANY HIGH SPEED ROAD (≥ 45 MPH), IT SHALL BE EQUIPPED WITH A DEPLOYED TRUCK/TRAILER MOUNTED IMPACT ATTENUATOR (TMIA, SEE TABLE 011-01 ON SHEET 619-011). BALLAST MAY BE USED TO BRING A LIGHTER VEHICLE UP TO THE INDICATED WEIGHT PROVIDED THE BALLAST IS SECURELY CONTAINED WITHIN AN ENCLOSED BODY OR OTHERWISE SECURELY FASTENED TO THE VEHICLE PURSUANT TO FEDERAL MOTOR CARRIER SAFETY ADMINISTRATION (FMCSA) CARGO SECUREMENT RULES, SUCH THAT:
 - 1) THE BALLAST WILL NOT SEPARATE FROM THE VEHICLE UPON IMPACT AND
 - 2) THE BALLAST WEIGHT WILL NOT EXCEED THE MANUFACTURER'S GROSS VEHICLE WEIGHT RATING (GVWR).TRUCK/TRAILER MOUNTED IMPACT ATTENUATORS SHALL NOT BE MOUNTED/INSTALLED ON VEHICLES WITH A GROSS WEIGHT (GVW) LESS THAN WHAT IS MINIMALLY REQUIRED BY THE MANUFACTURER OF THE TMIA.
3. WHEN A PROTECTIVE VEHICLE(S) IS USED BETWEEN THE WORK VEHICLE (CREW) OR HAZARD AND THE TRAFFIC IN A MOVING OPERATION IT IS REFERRED TO AS A SHADOW VEHICLE(S).
4. WHEN A PROTECTIVE VEHICLE(S) IS USED BETWEEN THE WORK VEHICLE (CREW) OR HAZARD AND THE TRAFFIC IN A STATIONARY OPERATION IT IS REFERRED TO AS A BARRIER VEHICLE(S).
5. WHEN A PROTECTIVE VEHICLE IS USED IN ADVANCE OF EITHER MOVING OR STATIONARY OPERATIONS TO DISPLAY SIGN MESSAGES IT IS REFERRED TO AS AN ADVANCE WARNING VEHICLE. WHEN SIGNS ARE MOUNTED ON AN ADVANCED WARNING VEHICLE, THEY SHALL NOT OBSTRUCT VISIBILITY OF ANY LIGHTS (TAILLIGHTS OR WARNING LIGHTS), OR SIDE-VIEW MIRRORS ON THE VEHICLE, OR TRUCK MOUNTED ARROW BOARDS.
6. IN A MOVING OPERATION OR A STATIONARY OPERATION THAT OCCUPIES A LOCATION FOR UP TO 1 HOUR, THE OPERATOR SHALL REMAIN IN THE PROTECTIVE VEHICLE WITH THE SAFETY BELT AND HEADREST PROPERLY ADJUSTED, MAINTAIN VEHICLE SPACING, AND KEEP THE WHEELS ALIGNED WITH THE LANE STRIPING AND LANE TO MAINTAIN LANE DISCIPLINE AND TO STAY IN LANE IF STRUCK. THE PARKING BRAKE SHALL BE SET WHENEVER POSSIBLE. TWO-WAY RADIOS SHOULD BE USED TO COMMUNICATE BETWEEN THE OPERATOR AND THE WORK CREW.
7. IN A STATIONARY OPERATION THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR, ONCE THE PROTECTIVE VEHICLE HAS BEEN APPROPRIATELY PLACED, IT SHOULD BE UNOCCUPIED. UNOCCUPIED VEHICLE SHALL BE POSITIONED PARALLEL TO TRAFFIC, PARKING BRAKE SET, PLACED IN 2ND GEAR (MANUAL TRANSMISSIONS /ENGINE OFF) OR PARK / NEUTRAL (AUTOMATIC TRANSMISSIONS). THE FRONT WHEELS SHALL BE ALIGNED WITH THE LANE STRIPING AND LANE TO MAINTAIN LANE DISCIPLINE AND TO STAY IN LANE IF STRUCK.
8. NO WORK ACTIVITY, EQUIPMENT, VEHICLES AND/OR MATERIALS SHALL BE LOCATED BETWEEN THE PROTECTIVE VEHICLE AND THE ACTIVE WORK AREA (ROLL AHEAD DISTANCE).
9. DIRECT VERBAL COMMUNICATION BETWEEN THE PROTECTIVE VEHICLES AND THE WORK VEHICLE(S) / EQUIPMENT SHALL BE UTILIZED WHERE AVAILABLE.

WORK DURATION DEFINITIONS

1. THERE ARE MAINLY FIVE WORK DURATIONS:
 - A. LONG-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN 3 CONSECUTIVE DAYS.
 - B. INTERMEDIATE-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN ONE DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK LASTING MORE THAN 1 HOUR.
 - C. SHORT-TERM IS STATIONARY DAYTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD.
 - D. SHORT DURATION IS WORK THAT OCCUPIES A LOCATION UP TO 1 HOUR. IT CAN BE PERFORMED DURING THE DAYTIME OR AT NIGHT IN ACCORDANCE WITH NOTES N1 TO N11 NOTES ON NIGHTTIME WORK.
 - E. MOBILE IS WORK THAT MOVES INTERMITTENTLY OR CONTINUOUSLY WHERE THE WORK AT ANY SPECIFIC LOCATION COMPLETES WITHIN 15 MINUTES. IT IS USED FOR VEHICLE BASED OPERATIONS AND DOES NOT INVOLVE WORKERS ON FOOT. IT CAN BE PERFORMED DURING THE DAYTIME OR AT NIGHT IN ACCORDANCE WITH NOTES N1 TO N10 NOTES ON NIGHTTIME WORK.
2. SPECIAL OPERATIONS ARE WORK OPERATIONS THAT DO NOT FIT INTO ONE OF THE ABOVE FIVE CATEGORIES. SPECIAL OPERATIONS INCLUDE:
 - A. STOP AND GO OPERATIONS - WORK THAT COMPLETES WITHIN 5 MINUTES AND ALLOWS WORKERS ON FOOT.
 - B. OTHER OPERATIONS INCLUDING MOWING, MULCHING/HERBICIDE OPERATIONS, TEMPORARY ROAD/INTERSECTION CLOSURES, ETC.

ROADWAY TYPE DEFINITIONS

1. FREEWAY:
 - A. INTERSTATE: INTERREGIONAL HIGH-SPEED, HIGH-VOLUME, DIVIDED FACILITIES WITH COMPLETE CONTROL OF ACCESS.
 - B. PARKWAY: DIVIDED HIGHWAYS FOR NON-COMMERCIAL TRAFFIC WITH FULL CONTROL OF ACCESS, GRADE PARKWAY SEPARATIONS, INTERCHANGES, AND OCCASIONAL AT-GRADE INTERSECTIONS. PARKWAYS ARE DESIGNATED BY LAW.
2. EXPRESSWAY: DIVIDED HIGHWAYS FOR THROUGH TRAFFIC WITH FULL OR PARTIAL CONTROL OF ACCESS AND GENERALLY WITH GRADE SEPARATIONS AT MAJOR CROSSROADS. ALL FREEWAY STANDARD SHEETS ARE APPLICABLE TO EXPRESSWAY.
3. NON-FREEWAY:
 - A. MULTILANE DIVIDED HIGHWAY
 - B. MULTILANE UNDIVIDED HIGHWAY
 - C. TWO-LANE TWO-WAY ROADWAY

ALL NON-FREEWAYS CAN BE EITHER URBAN OR RURAL:

URBAN: (MEETS MORE THAN 1 OF THE FOLLOWING CRITERIA)

- HIGH DENSITY DEVELOPMENT
- ON-STREET PARKING
- VARIED BUILDING SETBACKS
- MULTI-STORY AND LOW-TO MEDIUM-RISE STRUCTURES FOR RESIDENTIAL
- COMMERCIAL, AND EDUCATIONAL USES, STRUCTURES THAT ACCOMMODATE MIXED USES: COMMERCIAL, RESIDENTIAL, AND PARKING
- LIGHT INDUSTRIAL, AND SOMETIMES HEAVY INDUSTRIAL, LAND USE
- PROMINENT DESTINATIONS WITH SPECIALIZED STRUCTURES, E.G., LARGE THEATERS, SPORTS FACILITIES OR CONFERENCE CENTERS
- HIGH LEVELS OF PEDESTRIAN AND BICYCLIST ACTIVITY, WITH NEARLY CONTINUOUS SIDEWALKS AND MARKED CROSSWALKS
- HIGHER DENSITY OF TRANSIT STOPS AND ROUTES
- DRIVEWAY DENSITIES GREATER THAN 25 DRIVEWAYS/MILE ON EACH SIDE OF THE ROAD
- MINOR COMMERCIAL DRIVEWAY DENSITIES OF 10 DRIVEWAYS/MILE OR GREATER
- MAJOR COMMERCIAL DRIVEWAYS
- HIGH DENSITY OF CROSS STREETS

RURAL: DOES NOT MEET MORE THAN ONE OF THE ABOVE CRITERIA.

NOTES FOR NIGHTTIME OPERATIONS:

- N1. WORK OCCURRING AFTER SUNSET AND BEFORE SUNRISE WILL BE CONSIDERED NIGHTTIME OPERATIONS.
- N2. ALL SIGNS, STOP/SLOW PADDLES AND RED FLAGS USED TO WARN/ALERT/CONTROL TRAFFIC SHALL BE RETROREFLECTIVE.
- N3. ALL WORKERS INVOLVED SHALL WEAR PROTECTIVE HELMETS AND NIGHTTIME APPAREL IN ACCORDANCE WITH §107-05A. HIGH VISIBILITY APPAREL AT ALL TIMES.
- N4. VEHICLES OPERATING ON THE PAVEMENT OF A CLOSED ROADWAY OR TRAVEL LANE SHALL DISPLAY ROTATING AMBER BEACONS OR FLASHING LED BEACONS AT ALL TIMES.
- N5. LEVEL I ILLUMINATION SHALL BE PROVIDED NEAR THE BEGINNING OF LANE CLOSURE TAPERS AND AT ROAD CLOSURES, INCLUDING THE SETUP AND REMOVAL OF THE CLOSURE TAPERS.
- N6. LEVEL II ILLUMINATION SHALL BE PROVIDED FOR FLAGGING STATIONS, ASPHALT PAVING, MILLING, AND CONCRETE PLACEMENT AND/OR REMOVAL OPERATIONS, INCLUDING BRIDGE DECKS, 50 FEET AHEAD OF AND 100 FEET BEHIND A PAVING OR MILLING MACHINE.
- N7. LEVEL III ILLUMINATION SHALL BE PROVIDED FOR PAVEMENT OR STRUCTURAL CRACK FILLING, JOINT REPAIR, PAVEMENT PATCHING AND REPAIRS, INSTALLATION OF SIGNAL EQUIPMENT OR OTHER ELECTRICAL/MECHANICAL EQUIPMENT, AND OTHER TASKS INVOLVING FINE DETAILS OR INTRICATE PARTS AND EQUIPMENT.
- N8. ALL LIGHTING SHALL BE DESIGNED, INSTALLED, AND OPERATED TO AVOID GLARE THAT AFFECTS TRAFFIC ON THE ROADWAY OR THAT CAUSES ANNOYANCE OR DISCOMFORT FOR RESIDENCES ADJOINING THE ROADWAY.
- N9. PRIOR TO THE START OF NIGHTTIME OPERATIONS, A WRITTEN NIGHTTIME OPERATIONS AND LIGHTING PLAN IS REQUIRED FOR APPROVAL FROM THE DOT ENGINEER.
- N10. SEE STANDARD SPECIFICATIONS §619 FOR ADDITIONAL REQUIREMENTS AND CONSIDERATIONS. REFER TO SECTION 619-3.19B FOR BALLOON LIGHTING REQUIREMENTS.
- N11. FLAGGERS SHALL USE A FLASHLIGHT WITH RED GLOW CONE/RED LED BATON FOR FLAGGING IN NON-ILLUMINATED FLAGGER STATIONS DURING NIGHTTIME OPERATIONS.



Department of
Transportation

U.S. CUSTOMARY STANDARD SHEET

WORK ZONE TRAFFIC CONTROL
GENERAL NOTES

APPROVED DECEMBER 21, 2022

Robert Limoges
ROBERT LIMOGES, P.E.
DIRECTOR, OTSM

ISSUED UNDER EI 22-033

619-010

ERRATA 1 EFF. 05/01/2023
ISSUED WITH EB 22-033

TABLE 011-01: PROTECTIVE VEHICLE REQUIREMENTS																					
CLOSURE TYPE	DURATION	MOBILE OPERATION AND STOP & GO				SHORT DURATION OPERATION				SHORT TERM OPERATION				INTERMEDIATE TERM OPERATION				LONG TERM OPERATION			
	ROAD TYPE & SPEED	FREEWAY	NON-FREEWAY			FREEWAY	NON-FREEWAY			FREEWAY	NON-FREEWAY			FREEWAY	NON-FREEWAY			FREEWAY	NON-FREEWAY		
			≥ 45 MPH	35 - 40 MPH	≤ 30 MPH		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH
	EXPOSURE CONDITIONS (SEE NOTE 1)																				
LANE CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR WORK VEHICLE EXPOSED TO TRAFFIC	PVH+TMIA	PVH+TMIA	PVL+TMIA	PVL	PVH+TMIA	PVH+TMIA	PVL+TMIA	PVL	PVH+TMIA	PVH+TMIA	PVL+TMIA	PVL	PVH+TMIA	PVH+TMIA	PVL+TMIA	SEE NOTE 2	PVH+TMIA	PVH+TMIA	PVL+TMIA	SEE NOTE 2
	-NO WORKERS ON FOOT -NO WORK VEHICLE EXPOSED TO TRAFFIC -OTHER HAZARDS EXPOSED (IE EQUIPMENT, MATERIALS)					PVH+TMIA	PVH+TMIA	PVL	PVL	PVH+TMIA	PVH+TMIA	PVL	SEE NOTE 2	PVH+TMIA	PVH+TMIA	SEE NOTE 2	SEE NOTE 2	PVH+TMIA	PVH+TMIA	SEE NOTE 2	SEE NOTE 2
SHOULDER CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR WORK VEHICLE EXPOSED TO TRAFFIC	PVH+TMIA	PVH+TMIA	PVL+TMIA	PVL	PVH+TMIA	PVH+TMIA	PVL	PVL	PVH+TMIA	PVH+TMIA	PVL	PVL	PVH+TMIA	PVH+TMIA	SEE NOTE 2	SEE NOTE 2	PVH+TMIA	PVH+TMIA	SEE NOTE 2	SEE NOTE 2
	-NO WORKERS ON FOOT -NO WORK VEHICLE EXPOSED TO TRAFFIC -OTHER HAZARDS EXPOSED (IE EQUIPMENT, MATERIALS, EXCAVATION)					PVH+TMIA	PVH+TMIA	PVL	PVL	PVH+TMIA	PVH+TMIA	PVL	SEE NOTE 2	PVH+TMIA	SEE NOTE 3	SEE NOTE 2	SEE NOTE 2	PVH+TMIA	SEE NOTE 3	SEE NOTE 2	SEE NOTE 2
LEGEND PVL - PROTECTIVE VEHICLE LIGHT (MINIMUM GROSS WEIGHT 9,500 LBS. OR GREATER) (SEE NOTE 5) PVH - PROTECTIVE VEHICLE HEAVY (MINIMUM GROSS WEIGHT 22,000 LBS. OR GREATER) TMIA - TRUCK/TRAILER MOUNTED IMPACT ATTENUATOR											NOTES 1. THE EXPOSURE CONDITIONS ASSUME THERE IS NO POSITIVE PROTECTION PRESENT. 2. EITHER A PROTECTIVE LIGHT (PVL) OR THE STANDARD BUFFER SPACE (SEE TABLE 011-03 SHALL BE PROVIDED. 3. EITHER A PROTECTIVE VEHICLE HEAVY (PVH) OR THE STANDARD BUFFER SPACE (SEE TABLE 011-03) SHALL BE PROVIDED. 4. TRUCK/TRAILER MOUNTED IMPACT ATTENUATORS (TMIA) SHALL NOT BE MOUNTED/INSTALLED ON VEHICLES WITH A GROSS VEHICLE WEIGHT (GVW) LESS THAN WHAT IS MINIMALLY REQUIRED BY THE MANUFACTURER OF THE TMIA. 5. THE USE OF A PROTECTIVE VEHICLE LIGHT (PVL) AS A SHADOW VEHICLE IS LIMITED TO NON-FREEWAY ROADWAYS WHERE THE POSTED SPEED LIMITS IS ≤ 40 MPH UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.										

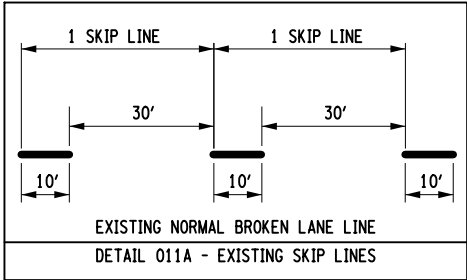


TABLE 011-02: TAPER LENGTHS & NUMBER OF CONES CHART												
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	TAPER LENGTH: L (FT.) / * OF SKIP LINES / * OF CHANNELIZING DEVICES									SHOULDER TAPER LENGTH: L/3 (FT.) / * OF SKIP LINES / * OF CHANNELIZING DEVICES		
	LATERAL SHIFT OF TRAFFIC FLOW PATH (FT.)									FOR SHOULDER WIDTH		
	4	5	6	7	8	9	10	11	12	≤ 4 FT.	5 - 7 FT.	≥ 8 FT.
25	40/1/2	80/2/3	80/2/3	80/2/3	80/2/3	120/3/4	120/3/4	120/3/4	120/3/4	40/1/2	40/1/2	40/1/2
30	80/2/3	80/2/3	80/2/3	120/3/4	120/3/4	160/4/5	160/4/5	160/4/5	200/5/6	40/1/2	40/1/2	40/1/2
35	80/2/3	120/3/4	120/3/4	160/4/5	160/4/5	200/5/6	200/5/6	240/6/7	240/6/7	40/1/2	40/1/2	80/2/3
40	120/3/4	160/4/5	160/4/5	200/5/6	240/6/7	240/6/7	280/7/8	320/8/9	320/8/9	40/1/2	80/2/3	80/2/3
45	200/5/6	240/6/7	280/7/8	320/8/9	360/9/10	400/10/11	440/11/12	520/13/14	560/14/15	80/2/3	80/2/3	120/3/4
50	200/5/6	240/6/7	320/8/9	360/9/10	400/10/11	440/11/12	520/13/14	560/14/15	600/15/16	80/2/3	120/3/4	160/4/5
55	240/6/7	280/7/8	320/8/9	400/10/11	440/11/12	520/13/14	560/14/15	600/15/16	680/17/18	80/2/3	120/3/4	160/4/5
60	240/6/7	320/8/9	360/9/10	440/11/12	480/12/13	560/14/15	600/15/16	680/17/18	720/18/19	80/2/3	120/3/4	160/4/5
65	280/7/8	320/8/9	400/10/11	480/12/13	520/13/14	600/15/16	640/16/17	720/18/19	800/19/20	80/2/3	160/4/5	200/5/6
* THIS TABLE WAS CREATED WITH REFERENCE TO MUTCD TABLE 6H-4. ** THE NUMBER OF CHANNELIZING DEVICES SHOWN IS CALCULATED BASED ON A 40FT DEVICE SPACING. THE NUMBER OF CHANNELIZING DEVICES CAN BE ADJUSTED AS NECESSARY.												

TABLE 011-03 LONGITUDINAL BUFFER SPACE	
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	DISTANCE (FT.) / * OF SKIP LINES
25	155 / 4
30	200 / 5
35	250 / 6
40	305 / 8
45	360 / 9
50	425 / 11
55	495 / 13
65	645 / 16
* THIS TABLE IS THE SAME AS MUTCD TABLE 6C-2.	

TABLE 011-04: ROLL AHEAD DISTANCE FOR PROTECTIVE VEHICLES					
ROLL AHEAD DISTANCE (FT.) / * OF SKIP LINES FOR VEHICLES					
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	PROTECTIVE VEHICLES WEIGHING 9,500 TO 21,999 LBS. GVW		PROTECTIVE VEHICLES WEIGHING 22,000 LBS. OR GREATER GVW		
	STATIONARY OPERATION	MOVING OPERATION (15 MPH MAX.)	STATIONARY OPERATION	MOVING OPERATION (15 MPH MAX.)	
≥ 60	200/5	240/6	160/4	200/5	
45 - 55	160/4	200/5	120/3	160/4	
≤ 40	120/3	120/3	80/2	120/3	

TABLE 011-05 FLARE RATES FOR POSITIVE BARRIER					
TYPE OF POSITIVE BARRIER	POSTED SPEED LIMIT				
	30 MPH	40 MPH	50 MPH	55 MPH	65 MPH
TEMPORARY POSITIVE BARRIER	8:1	11:1	14:1	16:1	20:1
BOX BEAM OR HEAVY POST CORRUGATED BEAM	7:1	9:1	11:1	12:1	15:1

TABLE 011-06 ADVANCE WARNING SIGN SPACING					
ROAD TYPE	DISTANCE BETWEEN SIGNS			SIGN LEGEND	
	A (FT.)	B (FT.)	C (FT.)	XX	YY
URBAN (≤ 30 MPH*)	100	100	100	AHEAD	AHEAD
URBAN (35-40 MPH*)	200	200	200	AHEAD	AHEAD
URBAN (≥ 45 MPH*)	350	350	350	1000 FT.	AHEAD
RURAL	500	500	500	1500 FT.	1000 FT.
FREEWAY	1000	1500	2640	1 MILE	1/2 MILE
* PRECONSTRUCTION POSTED SPEED LIMIT. ** THIS TABLE IS THE SAME AS MUTCD TABLE NY6H-3.					

TABLE 011-07 TAPER LENGTH FOR TEMPORARY TRAFFIC CONTROL ZONES	
TYPE OF TAPER	TAPER LENGTH (L)
MERGING TAPER	L
SHIFTING TAPER	L/2
SHOULDER TAPER	L/3
ONE-LANE, TWO-WAY TRAFFIC TAPER	50 FT. MIN -100 FT. MAX
DOWNSTREAM TAPER	50 FT. MIN -100 FT. MAX
* THIS TABLE IS THE SAME AS MUTCD TABLE 6C-3.	

WORK ZONE TRAFFIC CONTROL LEGEND	
SYMBOL	DESCRIPTION
	SIGN, TEMPORARY
	SPOTTER
	TEMPORARY POSITIVE BARRIER
	TEMPORARY POSITIVE BARRIER WITH WARNING LIGHTS
	TEMPORARY TRAFFIC SIGNAL HEAD
	TYPE III BARRICADE
	WARNING LIGHTS
	WORK AREA
	WORK VEHICLE
	WORK VEHICLE (MULCHING/HERBICIDE OPERATION)
	WORK VEHICLE (PAVEMENT MARKING)
	WORK VEHICLE (SIGNAL WORK)
	PROTECTIVE VEHICLE
	PROTECTIVE VEHICLE LIGHT
	PROTECTIVE VEHICLE HEAVY
	TRUCK/TRAILER MOUNTED IMPACT ATTENUATOR (TMIA)

WORK ZONE TRAFFIC CONTROL LEGEND	
SYMBOL	DESCRIPTION
	ARROW PANEL
	ARROW PANEL, CAUTION MODE
	ARROW PANEL TRAILER OR SUPPORT
	CHANGEABLE MESSAGE SIGN (PVMS)
	CHANNELIZING DEVICE
	CONE
	CRASH CUSHION/TEMPORARY IMPACT ATTENUATOR
	DIRECTION OF TEMPORARY TRAFFIC DETOUR
	DIRECTION OF TRAFFIC
	AUTOMATED FLAGGER ASSISTANCE DEVICE WITH OPERATOR
	FLAGGER
	FLAG TREE
	LUMINAIRE
	MOWER
	PARKWAY GRASS SHOULDER
	PAVEMENT MARKINGS THAT SHALL BE REMOVED FOR A LONG TERM PROJECT
	PORTABLE VARIABLE MESSAGE SIGN
	ADVANCE WARNING SIGN WITH ORANGE FLAGS
	TRAILER FOR ARROW PANEL OR PORTABLE VARIABLE MESSAGE SIGN (PVMS)

Department of Transportation	
U.S. CUSTOMARY STANDARD SHEET	
WORK ZONE TRAFFIC CONTROL GENERAL TABLES AND LEGEND	
APPROVED DECEMBER 21, 2022 <i>Robert Limoges</i> ROBERT LIMOGES, P.E. DIRECTOR, OTSM	ISSUED UNDER EI 22-033 619-011

WORK ZONE TRAFFIC CONTROL SIGN TABLE					
SIGN	SIGN DESIGNATION	COLOR CODE	CONVENTIONAL ROAD	EXPRESSWAY	FREEWAY
	E5-1	C	-----	72"x60"	72"x60"
	G20-1	A	36"x18"	48"x24"	48"x24"
	G20-2	A	36"x18"	48"x24"	48"x24"
	G20-4	A	36"x18"	-----	-----
	G20-5aP	A	24"x18"	36"x24"	36"x24"
	M1-1	G	1 OR 2 DIGITS 24"x24"	36"x36"	36"x36"
	M1-1+	G	3 DIGITS 30"x24"	45"x36"	45"x36"
	M1-4	B	1 OR 2 DIGITS 24"x24"	36"x36"	36"x36"
	M1-4+	B	3 DIGITS 30"x24"	45"x36"	45"x36"
	M3-1	SEE NOTE 3	24"x12"	36"x18"	36"x18"
	M3-2				
	M3-3				
	M3-4				
	M4-8	A	24"x12"	36"x18"	36"x18"
	M4-8a	A	24"x18"	24"x18"	24"x18"
	M4-9 M4-9L M4-9R	A	30"x24"	48"x36"	48"x36"
	M4-9a	A	30"x24"	30"x24"	-----
	M4-9b	A	30"x24"	30"x24"	-----
	M4-9c	A	30"x24"	30"x24"	-----
	M4-10L	A	48"x18"	48"x18"	48"x18"
	M4-10R				
	M5-1	SEE NOTE 3	21"x15"	30"x21"	30"x21"
	M5-2	SEE NOTE 3	21"x15"	30"x21"	30"x21"
	M6-1	SEE NOTE 3	21"x15"	30"x21"	30"x21"
	M6-2				
	M6-3				
	M6-4				
	NYM3-1	B	24"x24"	36"x36"	36"x36"
	NYM3-2	B	30"x24"	45"x36"	45"x36"
	NYM3-3	B	30"x24"	45"x36"	45"x36"

WORK ZONE TRAFFIC CONTROL SIGN TABLE					
SIGN	SIGN DESIGNATION	COLOR CODE	CONVENTIONAL ROAD	EXPRESSWAY	FREEWAY
	NYR9-11	B	24"x42"	48"x84"	48"x84"
	NYR9-12	B	24"x36"	36"x54"	48"x72"
	NYW4-17	A	36"x36"	48"x48"	48"x48"
	NYW8-30	A	48"x24"	48"x24"	48"x24"
	NYW8-31	A	48"x24"	48"x24"	48"x24"
	NYW8-32	A	48"x24"	48"x24"	48"x24"
	NYW8-33	A	48"x24"	48"x24"	48"x24"
	R1-1	D	36"x36"	36"x36"	48"x48"
	R1-2	E	36"x36"x36"	48"x48"x48"	60"x60"x60"
	R2-1	B	24"x30" OR 30"x36" (SEE NOTE 5)	36"x48"	36"x48"
	R2-11	B	24"x30"	36"x48"	36"x48"
	R2-12	B	24"x36"	36"x54"	36"x54"
	R4-1	B	24"x30"	36"x48"	36"x48"
	R4-7	B	24"x30"	36"x48"	36"x48"
	R4-7c NARROW	B	18"x30"	-----	-----
	R4-8	B	24"x30"	36"x48"	36"x48"
	R4-8c NARROW	B	18"x30"	-----	-----
	R4-9	B	24"x30"	36"x48"	36"x48"
	R5-1	E	36"x36"	36"x36"	48"x48"
	R9-8	B	36"x18"	36"x18"	-----
	R9-9	B	24"x12"	24"x12"	-----
	R9-10L R9-10R	B	24"x12"	24"x12"	-----
	R9-11L R9-11R	B	24"x18"	24"x18"	-----
	R9-11dL R9-11dR	B	24"x12"	24"x12"	-----
	R10-6	B	24"x36"	24"x36"	-----
	R11-2	B	48"x30"	48"x30"	48"x30"

WORK ZONE TRAFFIC CONTROL SIGN TABLE					
SIGN	SIGN DESIGNATION	COLOR CODE	CONVENTIONAL ROAD	EXPRESSWAY	FREEWAY
	R11-3a	B	60"x30"	60"x30"	-----
	W1-4L W1-4R	A	36"x36"	48"x48"	48"x48"
	W1-4bL W1-4bR	A	36"x36"	48"x48"	48"x48"
	W1-4cL W1-4cR	A	36"x36"	48"x48"	48"x48"
	W1-6L	A	48"x24"	60"x30"	60"x30"
	W1-6R	A			
	W1-8L	A (NO BORDER)	18"x24"	30"x36"	30"x36"
	W1-8R	A (NO BORDER)			
	W3-1	A ⁴	36"x36"	48"x48"	48"x48"
	W3-2	A ⁴	36"x36"	48"x48"	48"x48"
	W3-3	A ⁴	36"x36"	48"x48"	48"x48"
	W3-4	A	36"x36"	48"x48"	48"x48"
	W3-5	A ⁴	36"x36"	48"x48"	48"x48"
	W4-1L W4-1R	A	36"x36"	48"x48"	48"x48"
	W4-2L W4-2R	A	36"x36"	48"x48"	48"x48"

ROADWAY DEFINITIONS:

CONVENTIONAL ROAD - A STREET OR HIGHWAY OTHER THAN A FREEWAY, OR EXPRESSWAY.

EXPRESSWAY - A DIVIDED HIGHWAY WITH PARTIAL CONTROL OF ACCESS.

FREEWAY - A DIVIDED HIGHWAY WITH FULL CONTROL OF ACCESS.

COLOR CODE LEGEND	
CODE	DESCRIPTION
A	BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND
B	BLACK LEGEND AND BORDER ON A WHITE BACKGROUND
C	WHITE LEGEND AND BORDER ON A GREEN BACKGROUND
D	WHITE LEGEND AND BORDER ON A RED BACKGROUND
E	RED LEGEND AND BORDER ON A WHITE BACKGROUND
F	BLACK LEGEND AND BORDER ON A FLOURESCENT YELLOW GREEN BACKGROUND
G	WHITE LEGEND AND BORDER ON A BLUE AND RED BACKGROUND

NOTES:

- DIMENSIONS ARE SHOWN AS WIDTH X HEIGHT.
- FOR SIGNAGE NOT SHOWN ON THESE TABLES REFER TO THE M.U.T.C.D.
- COLORS FOR DIRECTION PLAQUES, ADVANCE TURN ARROWS, AND DIRECTIONAL ARROWS SHALL MATCH THE ROUTE OR INTERSTATE SIGN THAT THEY SUPPLEMENT AS PER THE M.U.T.C.D.
- MULTICOLORED SYMBOL IMPOSED ON SIGN WITH BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND.
- FOR R2-1 SIGN LARGER DIMENSIONS SHALL BE USED WHEN SIGN FACES MULTIPLE LANES ON A CONVENTIONAL ROAD.



STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION

U.S. CUSTOMARY STANDARD SHEET

SIGN TABLE
(SHEET 1 OF 2)





















APPROVED APRIL 1, 2012














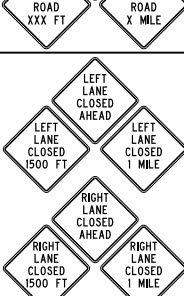
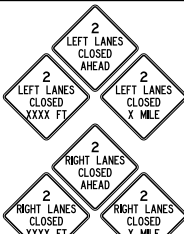

ISSUED UNDER EB 12-010





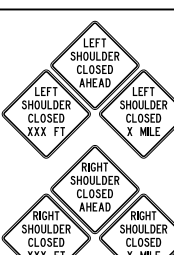





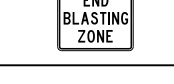


/S/ TODD WESTHUIS, P.E.
DIRECTOR, OFFICE OF
TRAFFIC SAFETY AND MOBILITY

619-12

EFFECTIVE DATE: 05/03/2012

WORK ZONE TRAFFIC CONTROL SIGN TABLE					
SIGN	SIGN DESIGNATION	COLOR CODE	CONVENTIONAL ROAD	EXPRESSWAY	FREEWAY
	W5-1	A	36"X36"	48"X48"	48"X48"
	W5-4	A	36"X36"	48"X48"	48"X48"
	W6-3	A	36"X36"	48"X48"	48"X48"
	W7-3aP	A	24"X18"	36"X30"	36"X30"
	W8-1	A	36"X36"	48"X48"	48"X48"
	W8-3	A	36"X36"	48"X48"	48"X48"
	W8-7	A	36"X36"	48"X48"	48"X48"
	W8-8	A	36"X36"	48"X48"	48"X48"
	W8-9	A	36"X36"	48"X48"	48"X48"
	W8-12	A	36"X36"	-----	-----
	W8-14	A	36"X36"	48"X48"	48"X48"
	W8-15	A	36"X36"	48"X48"	48"X48"
	W8-17	A	36"X36"	48"X48"	48"X48"
	W8-17p	A	24"X18"	30"X24"	30"X24"
	W8-23	A	36"X36"	48"X48"	48"X48"
	W8-24	A	36"X36"	48"X48"	48"X48"
	W9-3	A	36"X36"	48"X48"	48"X48"
	W11-1L W11-1R	A OR F	36"X36"	36"X36"	-----
	W11-2L W11-2R	F	36"X36"	36"X36"	-----
	W11-15L W11-15R	F	36"X36"	36"X36"	-----

WORK ZONE TRAFFIC CONTROL SIGN TABLE					
SIGN	SIGN DESIGNATION	COLOR CODE	CONVENTIONAL ROAD	EXPRESSWAY	FREEWAY
	W13-1P	A	24"X24"	30"X30"	30"X30"
	W13-4P	A	36"X36"	36"X36"	36"X36"
	W14-3	A	48"X48"X36"	-----	-----
	W16-1P	SEE NOTE 3 A OR F	18"X24"	24"X30"	-----
	W16-2P	A	24"X18"	30"X24"	-----
	W16-4P	SEE NOTE 3 A OR F	30"X24"	-----	-----
	W16-5PL W16-5PR	A	24"X18"	-----	-----
	W16-7PL W16-7PR	SEE NOTE 3 A OR F	24"X12"	30"X18"	-----
	W16-9P	SEE NOTE 3 A OR F	24"X12"	30"X18"	-----
	W20-1	A	36"X36"	48"X48"	48"X48"
	W20-2	A	36"X36"	48"X48"	48"X48"
	W20-3	A	36"X36"	48"X48"	48"X48"
	W20-4	A	36"X36"	48"X48"	48"X48"
	W20-5	A	36"X36"	48"X48"	48"X48"
	W20-5a	A	36"X36"	48"X48"	48"X48"
	W20-7	A	36"X36"	48"X48"	48"X48"

WORK ZONE TRAFFIC CONTROL SIGN TABLE					
SIGN	SIGN DESIGNATION	COLOR CODE	CONVENTIONAL ROAD	EXPRESSWAY	FREEWAY
	W21-1	A	36"X36"	48"X48"	48"X48"
	W21-4	A	36"X18"	48"X24"	48"X24"
	W21-5	A	36"X36"	48"X48"	48"X48"
	W21-5aL W21-5aR	A	36"X36"	48"X48"	48"X48"
	W21-5bL W21-5bR	A	36"X36"	48"X48"	48"X48"
	W21-8	A	36"X36"	48"X48"	48"X48"
	W22-1	A	36"X36"	48"X48"	48"X48"
	W22-2	A	42"X36"	42"X36"	42"X36"
	W22-3	A	42"X36"	42"X36"	42"X36"
	W23-2	A	36"X36"	48"X48"	48"X48"
	W24-1L W24-1R	A	36"X36"	48"X48"	48"X48"
	W24-1aL W24-1aR	A	36"X36"	48"X48"	48"X48"
	W24-1bL W24-1bR	A	36"X36"	48"X48"	48"X48"

ROADWAY DEFINITIONS:

CONVENTIONAL ROAD - A STREET OR HIGHWAY OTHER THAN A FREEWAY, OR EXPRESSWAY.

EXPRESSWAY - A DIVIDED HIGHWAY WITH PARTIAL CONTROL OF ACCESS.

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COLOR CODE LEGEND	
CODE	DESCRIPTION
A	BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND
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NOTES:

- DIMENSIONS ARE SHOWN AS WIDTH X HEIGHT.
- FOR SIGNAGE NOT SHOWN ON THESE TABLES REFER TO THE M.U.T.C.D.
- WHEN USED IN CONJUNCTION WITH A BICYCLE SIGN (W11-1) OR PEDESTRIAN CROSSING (W11-2) COLOR CODE SHALL MATCH.



Department of
Transportation

U.S. CUSTOMARY STANDARD SHEET

SIGN TABLE
(SHEET 2 OF 2)

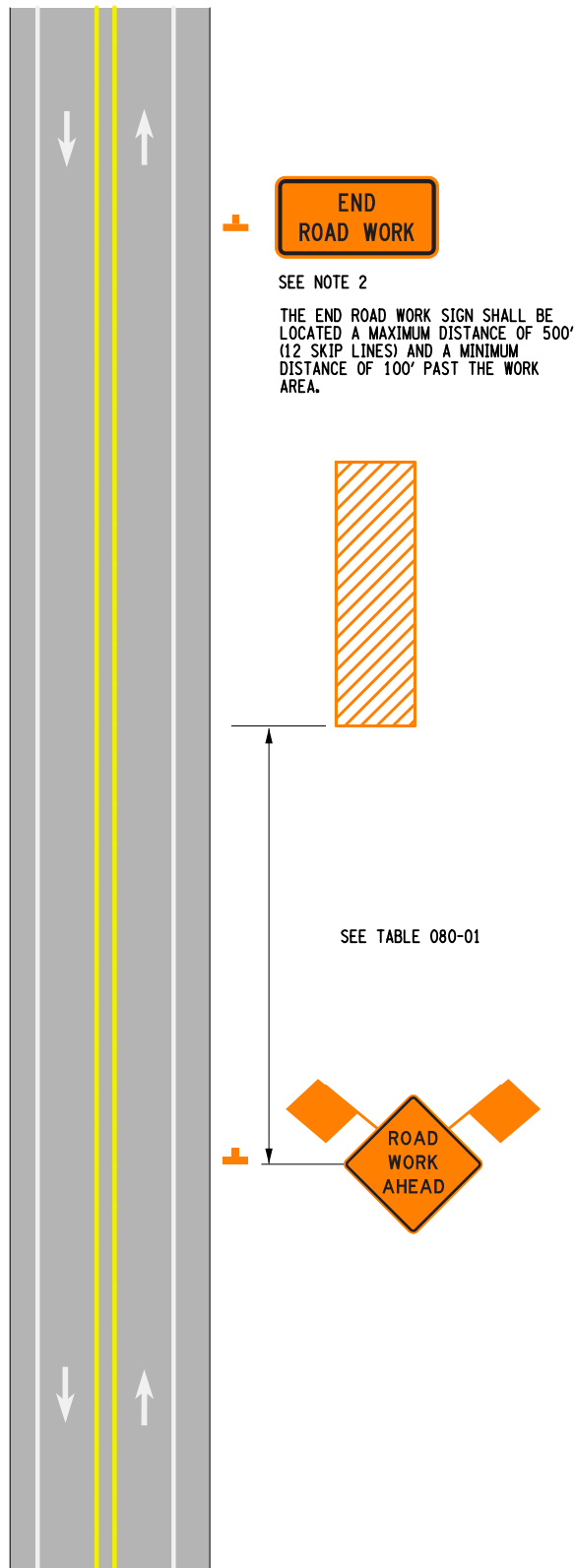
APPROVED APRIL 1, 2012

ISSUED UNDER EB 12-010

/S/ TODD WESTHUIS, P.E.
DIRECTOR, OFFICE OF
TRAFFIC SAFETY AND MOBILITY

619-12

ERRATA 1 EFF 01/01/2021
ISSUED WITH EB 20-048



NOT TO SCALE

NOTES:

1. THIS SETUP IS A SPECIAL OPERATION, AND CAN BE USED REGARDLESS OF THE WORK DURATION WHEN WORK IS PERFORMED BEYOND THE SHOULDERS WITHIN THE RIGHT-OF-WAY.
2. END ROAD WORK SIGN MAY BE OMITTED IF WORK DURATION IS LESS THAN 1 HOUR.

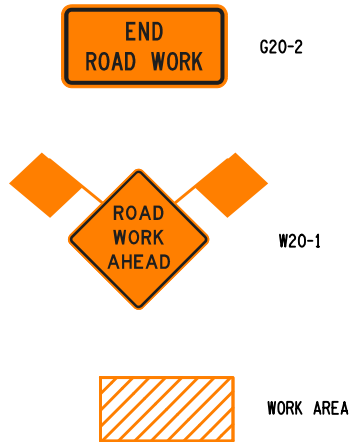


TABLE 080-01: ADVANCE PLACEMENT SIGN DISTANCE

ROAD TYPE	DISTANCE BETWEEN SIGNS
URBAN (≤ 30 MPH*)	100
URBAN (35-40 MPH*)	200
URBAN (≥ 45 MPH*)	350
RURAL	500
FREEWAY	1000

* PRECONSTRUCTION POSTED SPEED LIMIT

TABLE 080-02: REQUIRED SIGN SIZES*

SIGN	NON-FREEWAY	FREEWAY
G20-1	36x18	48x24
W20-1	36x36	48x48
WARNING FLAG	18x18	18x18

*FREEWAY SIZES MAY BE USED ON NON-FREEWAY, IF SPACE CONSTRAINTS DO NOT EXIST.



Department of
Transportation

U.S. CUSTOMARY STANDARD SHEET

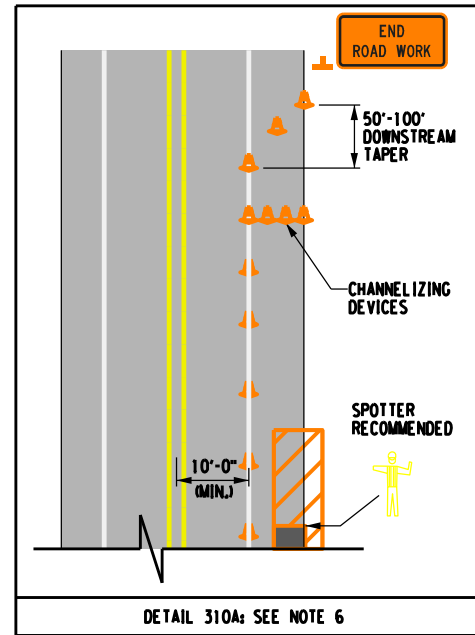
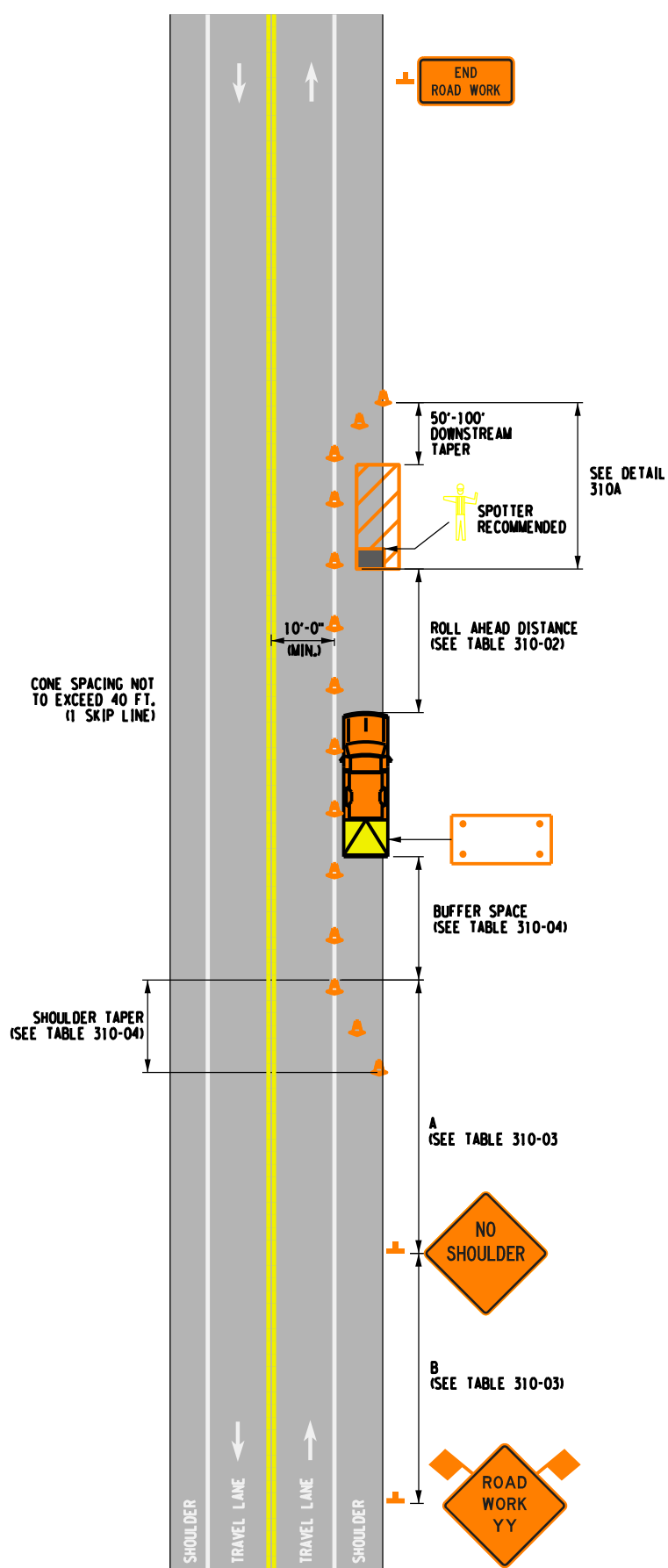
WORK ZONE TRAFFIC CONTROL
ALL ROADWAYS
WORK BEYOND SHOULDER

APPROVED DECEMBER 2, 2021

Robert Limoges
ROBERT LIMOGES, P.E.
DIRECTOR, OTSM

ISSUED UNDER EI 21-028

619-080



- NOTES:
1. SHORT-TERM STATIONARY IS DAYTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD.
 2. THE PROTECTIVE VEHICLE(S) SHALL MAINTAIN THE APPROPRIATE ROLL AHEAD DISTANCE. BE AN UNOCCUPIED TRUCK POSITIONED PARALLEL TO TRAFFIC, PARKING BRAKE SET, PLACED IN 2ND GEAR (MANUAL TRANSMISSIONS /ENGINE OFF) OR PARK / NEUTRAL (AUTOMATIC TRANSMISSIONS) AND HAVE THE FRONT WHEELS ALIGNED WITH THE LANE STRIPING.
 3. THERE SHALL BE NO WORKERS, EQUIPMENT OR OTHER VEHICLES IN THE BUFFER SPACE OR THE ROLL AHEAD DISTANCE.
 4. XX IS THE EXPECTED OVERALL LENGTH OF THE OPERATION TO BE COMPLETED WITHIN THE WORK DAY. A SUPPLEMENTAL DISTANCE PLAQUE W7-3a SHALL BE USED WITH SIGN W20-1 WHEN THE DISTANCE BETWEEN THE ADVANCE WARNING SIGNS AND WORK MAY BECOME GREATER THAN 2 MILES AS A RESULT OF THE FOLLOWING SITUATIONS:
•MULTIPLE WORK LOCATIONS ARE ANTICIPATED WITHIN XX MILES FROM THE W20-1 SIGN
•WORK AREA WILL BE RELOCATED DURING THE DURATION OF THE WORK WITHIN XX MILES FROM THE W20-1 SIGN.
THE SUPPLEMENT SIGN W7-3a SHALL INDICATE THE MAXIMUM ANTICIPATED DISTANCE BETWEEN THE W20-1 SIGN AND THE FARTHEST WORK LOCATION.
 5. WHEN MULTIPLE WORK LOCATIONS EXIST WITHIN XX MILES FROM THE W20-1 SIGN, A G20-1 SIGN SHALL BE PLACED EVERY TWO MILES INDICATING THE DISTANCE FROM THE SIGN TO THE FARTHEST WORK LOCATION.
 6. CHANNELIZING DEVICES SHALL BE PLACED TRANSVERSELY A MINIMUM OF EVERY 800' AS SHOWN WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 800'.
 7. IN URBAN CONDITIONS, ADVANCE WARNING SIGN SPACINGS MAY BE ADJUSTED IN ORDER TO ACCOMMODATE SIDE STREETS AND DRIVEWAYS. IF THERE IS A CONFLICT, MOVE THE SIGN UPSTREAM.

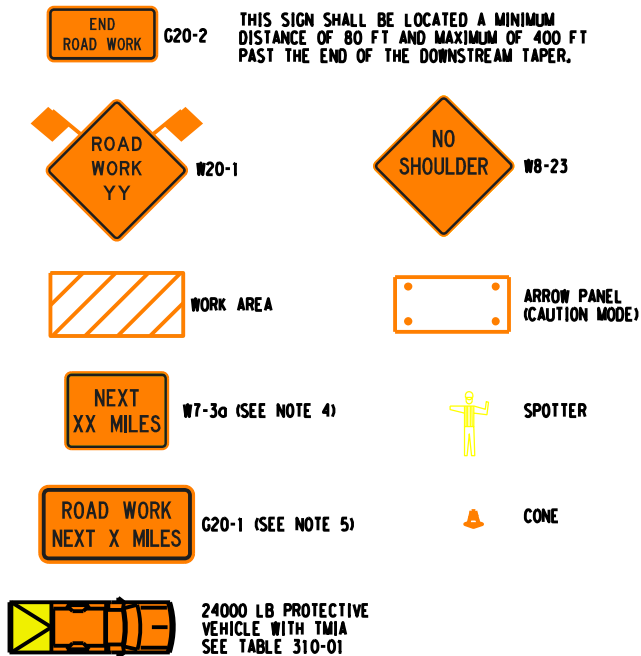


TABLE 310-05: REQUIRED SIGN SIZES*

SIGN	NON-FREEWAY	FREEWAY
G20-1	36x18	48x24
G20-2	36x18	48x24
W7-3a	24x18	36x30
W8-23	36x36	48x48
W20-1	36x36	48x48
WARNING FLAG	18x18	18x18

*FREEWAY SIZES MAY BE USED ON NON-FREEWAY, IF SPACE CONSTRAINTS DO NOT EXIST.

TABLE 310-01: PROTECTIVE VEHICLE REQUIREMENTS

CLOSURE TYPE	ROAD TYPE & SPEED	NON-FREEWAY		
		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH
LANE CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	P, TMIA	P
	OTHER HAZARDS NO WORKERS EXPOSED	P, TMIA	P	SEE NOTE 2
SHOULDER CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	P	P
	OTHER HAZARDS NO WORKERS EXPOSED	P, TMIA	P	SEE NOTE 2

LEGEND

P: PROTECTIVE VEHICLE REQUIRED FOR EACH CLOSED LANE & EACH CLOSED PAVED SHOULDER 8' OR WIDER. IF THE WORK SPACE MOVES WITHIN THE STATIONARY CLOSURE, THE PROTECTIVE VEHICLE SHALL BE REPOSITIONED ACCORDINGLY

TMIA: TMIA REQUIRED

NOTES:

1. THE EXPOSURE CONDITIONS ASSUME THERE IS NO POSITIVE PROTECTION PRESENT
2. EITHER A PROTECTIVE VEHICLE OR THE STANDARD BUFFER SPACE SHALL BE PROVIDED

TABLE 310-02: ROLL AHEAD DISTANCE

ROLL AHEAD DISTANCE (FT.) ^a OF SKIP LINES FOR VEHICLES		
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	STATIONARY OPERATION	
	MIN	MAX
≥ 55	120/3	200/5
45 - 50	80/2	160/4
≤ 40	40/1	120/3

TABLE 310-03: ADVANCE WARNING SIGN SPACING

ROAD TYPE	DISTANCE BETWEEN SIGNS		SIGN LEGEND	
	A (FT.)	B (FT.)	XX	YY
URBAN (≤ 30 MPH ^a)	100	100	AHEAD	AHEAD
URBAN (35-40 MPH ^a)	200	200	AHEAD	AHEAD
URBAN (≥ 45 MPH ^a)	350	350	1000 FT.	AHEAD
RURAL	500	500	1500 FT.	1000 FT.

^a PRECONSTRUCTION POSTED SPEED LIMIT

TABLE 310-04: LONGITUDINAL BUFFER SPACE AND TAPER LENGTHS

PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	LONGITUDINAL BUFFER SPACE DISTANCE (FT.) ^a OF SKIP LINES	TAPER LENGTH: L (FT.) ^a OF SKIP LINES/ ^a OF CHANNELIZING DEVICES			SHOULDER TAPER LENGTH: L/3 (FT.) ^a OF SKIP LINES/ ^a OF CHANNELIZING DEVICES		
		FOR LANE WIDTH IN FT. (LATERAL SHIFT OF TRAFFIC FLOW PATH)			FOR SHOULDER WIDTH		
		10	11	12	≤ 4 FT.	5 - 7 FT.	≥ 8 FT.
25	155/4	120/3/4	120/3/4	120/3/4	40/1/2	40/1/2	40/1/2
30	200/5	160/4/5	160/4/5	200/5/6	40/1/2	40/1/2	40/1/2
35	250/6	200/5/6	240/6/7	240/6/7	40/1/2	40/1/2	80/2/3
40	305/8	280/7/8	320/8/9	320/8/9	40/1/2	80/2/3	80/2/3
45	360/9	440/11/12	520/13/14	560/14/15	80/2/3	80/2/3	120/3/4
50	425/11	520/13/14	560/14/15	600/15/16	80/2/3	120/3/4	160/4/5
55	495/13	560/14/15	600/15/16	680/17/18	80/2/3	120/3/4	160/4/5



Department of Transportation

U.S. CUSTOMARY STANDARD SHEET

WORK ZONE TRAFFIC CONTROL
NON-FREEWAY
SHOULDER CLOSURE
SHORT TERM OPERATION

APPROVED DECEMBER 2, 2021

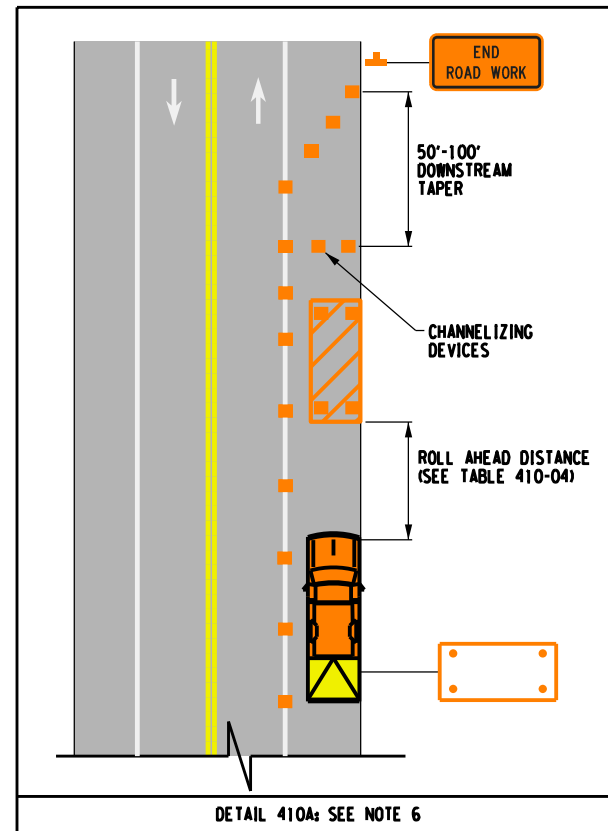
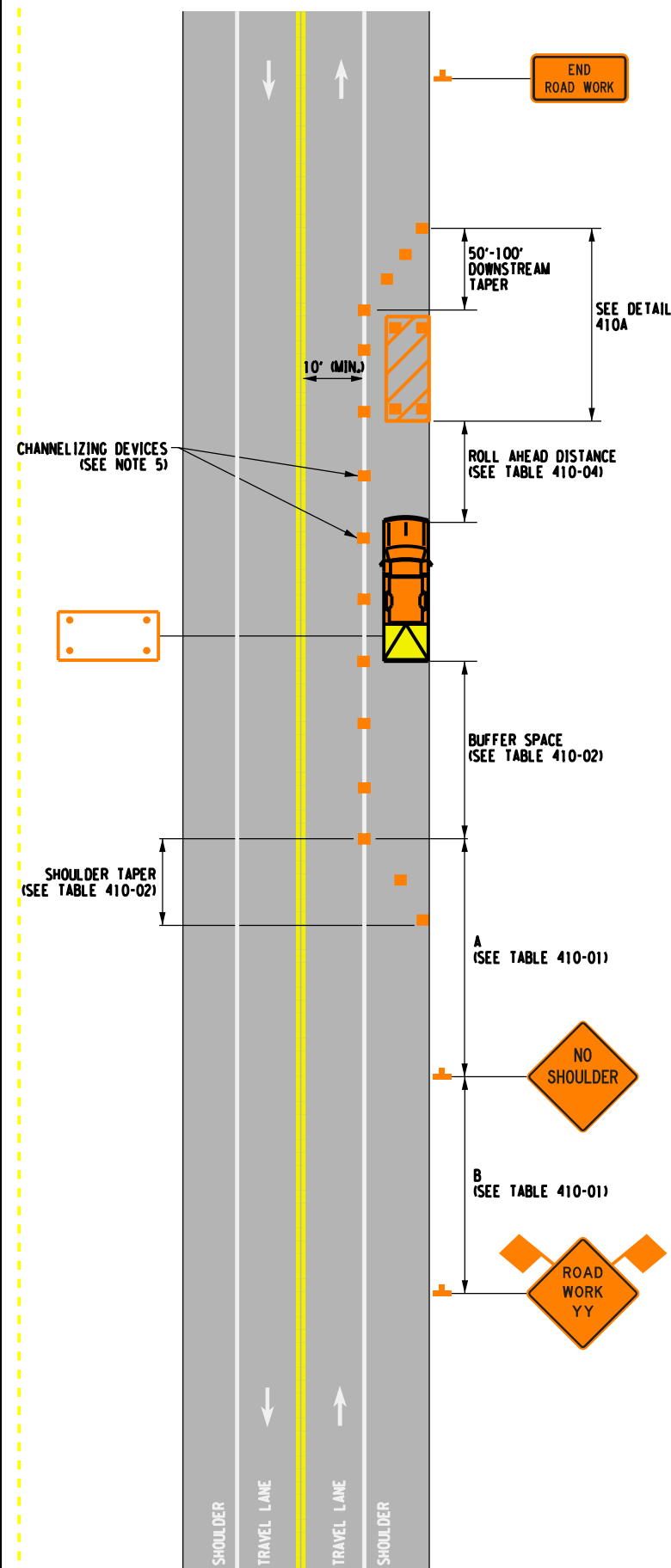
ISSUED UNDER EI 21-028

Robert Limoges
ROBERT LIMOGES, P.E.
DIRECTOR, OTSM

619-310

ERRATA 1 EFF. 01/01/23
ISSUED WITH EB 22-050

NOT TO SCALE

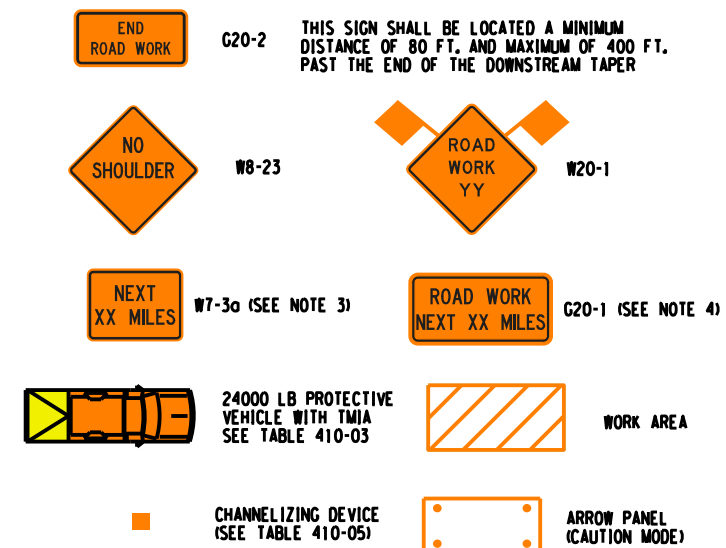


- NOTES:**

1. INTERMEDIATE-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN ONE DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK LASTING MORE THAN 1 HOUR.
2. NO WORK ACTIVITY OR STORAGE OF EQUIPMENT, VEHICLES, OR MATERIAL SHOULD OCCUR WITHIN A BUFFER SPACE.
3. XX IS THE EXPECTED OVERALL LENGTH OF THE OPERATION TO BE COMPLETED WITHIN THE WORK DAY. A SUPPLEMENTAL DISTANCE PLAQUE W7-3a SHALL BE USED WITH SIGN W20-1 WHEN THE DISTANCE BETWEEN THE ADVANCE WARNING SIGNS AND WORK MAY BECOME GREATER THAN 2 MILES AS A RESULT OF THE FOLLOWING SITUATIONS:
 - MULTIPLE WORK LOCATIONS ARE ANTICIPATED WITHIN XX MILES FROM THE W20-1 SIGN
 - WORK AREA WILL BE RELOCATED DURING THE DURATION OF THE WORK WITHIN XX MILES FROM THE W20-1 SIGN
- THE SUPPLEMENT SIGN W7-3a SHALL INDICATE THE MAXIMUM ANTICIPATED DISTANCE BETWEEN THE W20-1 SIGN AND THE FARTHEST WORK LOCATION.
4. WHEN MULTIPLE WORK LOCATIONS EXIST WITHIN XX MILES FROM THE W20-1 SIGN, A G20-1 SIGN SHALL BE PLACED EVERY TWO MILES INDICATING THE DISTANCE FROM THE SIGN TO THE FARTHEST WORK LOCATION.
5. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 20' IN THE ACTIVE WORK SPACE.
6. CHANNELIZING DEVICES SHALL BE PLACED TRANSVERSELY A MINIMUM OF EVERY 800' AS SHOWN WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 800'.
7. THE PROTECTIVE VEHICLE(S) SHALL MAINTAIN THE APPROPRIATE ROLL AHEAD DISTANCE, BE AN UNOCCUPIED TRUCK POSITIONED PARALLEL TO TRAFFIC, PARKING BRAKE SET, PLACED IN 2ND GEAR (MANUAL TRANSMISSIONS /ENGINE OFF) OR PARK / NEUTRAL (AUTOMATIC TRANSMISSIONS) AND HAVE THE FRONT WHEELS ALIGNED WITH THE LANE STRIPING.

- NOTES FOR NIGHTTIME OPERATIONS:**

- N1. WORK OCCURRING AFTER SUNSET AND BEFORE SUNRISE WILL BE CONSIDERED NIGHTTIME OPERATIONS.
- N2. ALL SIGNS, STOP/SLOW PADDLES AND RED FLAGS USED TO WARN/ALERT/CONTROL TRAFFIC SHALL BE RETROREFLECTIVE.
- N3. ALL WORKERS INVOLVED SHALL WEAR PROTECTIVE HELMETS AND NIGHTTIME APPAREL IN ACCORDANCE WITH §107-05A. HIGH VISIBILITY APPAREL AT ALL TIMES.
- N4. VEHICLES OPERATING ON THE PAVEMENT OF A CLOSED ROADWAY OR TRAVEL LANE SHALL DISPLAY ROTATING AMBER BEACONS OR FLASHING LED BEACONS AT ALL TIMES.
- N5. LEVEL I ILLUMINATION SHALL BE PROVIDED NEAR THE BEGINNING OF LANE CLOSURE TAPERS AND AT ROAD CLOSURES, INCLUDING THE SETUP AND REMOVAL OF THE CLOSURE TAPERS.
- N6. LEVEL II ILLUMINATION SHALL BE PROVIDED FOR FLAGGING STATIONS, ASPHALT PAVING, MILLING, AND CONCRETE PLACEMENT AND/OR REMOVAL OPERATIONS, INCLUDING BRIDGE DECKS, 50 FEET AHEAD OF AND 100 FEET BEHIND A PAVING OR MILLING MACHINE.
- N7. LEVEL III ILLUMINATION SHALL BE PROVIDED FOR PAVEMENT OR STRUCTURAL CRACK FILLING, JOINT REPAIR, PAVEMENT PATCHING AND REPAIRS, INSTALLATION OF SIGNAL EQUIPMENT OR OTHER ELECTRICAL/MECHANICAL EQUIPMENT, AND OTHER TASKS INVOLVING FINE DETAILS OR INTRICATE PARTS AND EQUIPMENT.
- N8. ALL LIGHTING SHALL BE DESIGNED, INSTALLED, AND OPERATED TO AVOID GLARE THAT AFFECTS TRAFFIC ON THE ROADWAY OR THAT CAUSES ANNOYANCE OR DISCOMFORT FOR RESIDENCES ADJOINING THE ROADWAY.
- N9. PRIOR TO THE START OF NIGHTTIME OPERATIONS, A WRITTEN NIGHTTIME OPERATIONS AND LIGHTING PLAN IS REQUIRED FOR APPROVAL FROM THE DOT ENGINEER.
- N10. SEE STANDARD SPECIFICATIONS §619 FOR ADDITIONAL REQUIREMENTS AND CONSIDERATIONS.
- N11. FLAGGERS SHALL USE A FLASHLIGHT WITH RED GLOW CONE/RED LED BATON FOR FLAGGING IN NON-ILLUMINATED FLAGGER STATIONS DURING NIGHTTIME OPERATIONS.



REFER TO SHEET 2 OF 2 FOR ALL TABLES

Department of
Transportation

U.S. CUSTOMARY STANDARD SHEET

**WORK ZONE TRAFFIC CONTROL
TWO-LANE TWO-WAY ROADWAY
SHOULDER CLOSURE
INTERMEDIATE TERM OPERATION
(SHEET 1 OF 2)**

APPROVED DECEMBER 2, 2021

Robert Limoges
ROBERT LIMOGES, P.E.
DIRECTOR, OTSM

ISSUED UNDER EI 21-028

619-410

ERRATA 1 EFF. 01/01/23
ISSUED WITH EB 22-050

NOT TO SCALE

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TABLE 410-01: ADVANCE WARNING SIGN SPACING		
ROAD TYPE	DISTANCE BETWEEN SIGNS	
	A (FT.)	B (FT.)
URBAN (≤ 30 MPH*)	100	100
URBAN (35-40 MPH*)	200	200
URBAN (≥ 45 MPH*)	350	350
RURAL	500	500
* PRECONSTRUCTION POSTED SPEED LIMIT		

TABLE 410-02: LONGITUDINAL BUFFER SPACE AND TAPER LENGTHS							
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	LONGITUDINAL BUFFER SPACE DISTANCE (FT.)/ * OF SKIP LINES	TAPER LENGTH: L (FT.)/ * OF SKIP LINES/ * OF CHANNELIZING DEVICES			SHOULDER TAPER LENGTH: L/3 (FT.)/ * OF SKIP LINES/ * OF CHANNELIZING DEVICES		
		FOR LANE WIDTH IN FT. (LATERAL SHIFT OF TRAFFIC FLOW PATH)			FOR SHOULDER WIDTH		
		10	11	12	≤ 4 FT.	5 - 7 FT.	≥ 8 FT.
25	155/4	120/3/4	120/3/4	120/3/4	40/1/2	40/1/2	40/1/2
30	200/5	160/4/5	160/4/5	200/5/6	40/1/2	40/1/2	40/1/2
35	250/6	200/5/6	240/6/7	240/6/7	40/1/2	40/1/2	80/2/3
40	305/8	280/7/8	320/8/9	320/8/9	40/1/2	80/2/3	80/2/3
45	360/9	440/11/12	520/13/14	560/14/15	80/2/3	80/2/3	120/3/4
50	425/11	520/13/14	560/14/15	600/15/16	80/2/3	120/3/4	160/4/5
55	495/13	560/14/15	600/15/16	680/17/18	80/2/3	120/3/4	160/4/5

TABLE 410-03: PROTECTIVE VEHICLE REQUIREMENTS				
CLOSURE TYPE	ROAD TYPE & SPEED	NON-FREEWAY		
		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH
	EXPOSURE CONDITIONS ¹			
LANE CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	P, TMIA	SEE NOTE 2
	OTHER HAZARDS NO WORKERS EXPOSED	P, TMIA	SEE NOTE 2	SEE NOTE 2
SHOULDER CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	SEE NOTE 2	SEE NOTE 2
	OTHER HAZARDS NO WORKERS EXPOSED	SEE NOTE 2	SEE NOTE 2	SEE NOTE 2
LEGEND				
P: PROTECTIVE VEHICLE REQUIRED FOR EACH CLOSED LANE & EACH CLOSED PAVED SHOULDER 8' OR WIDER, IF THE WORK SPACE MOVES WITHIN THE STATIONARY CLOSURE, THE PROTECTIVE VEHICLE SHALL BE REPOSITIONED ACCORDINGLY				
TMIA: TMIA REQUIRED				
NOTES:				
1. THE EXPOSURE CONDITIONS ASSUME THERE IS NO POSITIVE PROTECTION PRESENT				
2. EITHER A PROTECTIVE VEHICLE OR THE STANDARD BUFFER SPACE SHALL BE PROVIDED				

TABLE 410-04: ROLL AHEAD DISTANCE		
ROLL AHEAD DISTANCE (FT.)/ * OF SKIP LINES FOR VEHICLES		
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	STATIONARY OPERATION	
	MIN	MAX
≥ 55	120/3	200/5
45 - 50	80/2	160/4
≤ 40	40/1	120/3

TABLE 410-05: CHANNELIZING DEVICE APPLICATION FOR INTERMEDIATE-TERM STATIONARY WORK ZONES										
WORK ZONE PROVISIONS	MAXIMUM DEVICE SPACING (CENTER TO CENTER)	MUTCD COMPLIANT CHANNELIZING DEVICE								
		DRUMS	STANDARD CONES	TALL CONES	EXTRA TALL CONES	TEMPORARY TUBULAR MARKERS	INTERIM TUBULAR MARKERS	VERTICAL PANELS	OVERSIZED VERTICAL PANELS	TYPE III BARRICADES
INTERMEDIATE-TERM STATIONARY WORK ZONES INVOLVE WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR	20 FT. •	X							X	
	40 FT.	X							X	
MARKING FOR TRANSVERSE BUMPS ¹	N/A	X ²	X		X ²				X ²	
TRANSVERSE DEVICE WITHIN CLOSED TRAFFIC LANE AND/OR SHOULDER	800 FT.	X		X	X			X	X	0
REMOVAL OF EXISTING GUIDE RAIL	80 FT.	X		X	X	X		X	X	0
	40 FT.									
NOTES: X= ALLOWED, BLANK = NOT ALLOWED, 0 = OPTIONAL										
1. - A TYPE 1 OBJECT MARKER MAY BE USED IN LIEU OF CHANNELIZING DEVICE.										
2. - CHANNELIZING DEVICES SHALL BE EQUIPPED WITH A FLASHING WARNING LIGHT.										
• SEE NOTE 5 ON SHEET 1 OF 2.										

TABLE 410-06: REQUIRED SIGN SIZES*		
SIGN	NON-FREEWAY	FREEWAY
G20-2	36x18	48x24
W20-1	36x36	48x48
W21-5	36x36	48x48
W7-3a	24x18	36x30
G20-1	36x18	48x24
WARNING FLAG	18x18	18x18
*FREEWAY SIZES MAY BE USED ON NON-FREEWAY, IF SPACE CONSTRAINTS DO NOT EXIST.		



NEW YORK
STATE OF
OPPORTUNITY.

**Department of
Transportation**

U.S. CUSTOMARY STANDARD SHEET

WORK ZONE TRAFFIC CONTROL
TWO-LANE TWO-WAY ROADWAY
SHOULDER CLOSURE
INTERMEDIATE TERM OPERATION
(SHEET 2 OF 2)

APPROVED APRIL 8, 2022

ISSUED UNDER EI 22-008

Robert Limoges
ROBERT LIMOGES, P.E.
DIRECTOR, OTSM


619-410

- NOTES:
1. INTERMEDIATE-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN ONE DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK LASTING MORE THAN 1 HOUR.
 2. NO WORK ACTIVITY, EQUIPMENT, OR STORAGE OF VEHICLES, OR MATERIAL SHALL OCCUR WITHIN THE BUFFER SPACE AT ANY TIME.
 3. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 20' IN THE ACTIVE WORK SPACE AND THE TURN LANE TAPER SPACE.
 4. CHANNELIZING DEVICES SHALL BE PLACED TRANSVERSELY A MINIMUM OF EVERY 800' AS SHOWN WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 800'.
 5. WHERE THE SHIFTED SECTION IS LONGER THAN 600', ONE REVERSE CURVE SIGN SHOULD BE USED TO SHOW THE INITIAL SHIFT AND A SECOND SIGN SHOULD BE USED TO SHOW THE RETURN TO THE NORMAL ALIGNMENT. IF THE TANGENT DISTANCE ALONG THE TEMPORARY DIVERSION IS LESS THAN 600', THE DOUBLE REVERSE CURVE SIGN (W24-1L) SHOULD BE USED INSTEAD OF THE FIRST REVERSE CURVE SIGN (W1-4L) AND THE SECOND REVERSE CURVE SIGN (W1-4L) SHOULD BE OMITTED.
 6. THE ENGINEER MAY REQUIRE THE USE OF AN ADVISORY SPEED PLAQUE (W13-1) TO SUPPLEMENT A WARNING SIGN. THE PLAQUE WILL BE USED TO INDICATE AN ADVISORY SPEED FOR THE WORK ZONE CONDITION, (IE. NARROW LANES, BUMPS, POOR ROADWAY SURFACE, LOW OR NO SHOULDER, DROP-OFFS, GEOMETRIC CONSTRAINTS, AND/OR POOR SIGHT CONDITIONS).
 7. THE NYR9-11 SIGN IS RECOMMENDED. WHEN USED, IT SHALL BE PLACED IN ADVANCE OF THE FIRST ADVANCE WARNING SIGN. THE PLACEMENT DISTANCE SHALL BE 1000' FOR POSTED SPEED LIMITS OF 45 MPH OR HIGHER, AND 300' - 500' FOR POSTED SPEED LIMITS OF LESS THAN 45 MPH.
 8. THE PROTECTIVE VEHICLE(S) SHALL MAINTAIN THE APPROPRIATE ROLL AHEAD DISTANCE, BE AN UNOCCUPIED TRUCK POSITIONED PARALLEL TO TRAFFIC, PARKING BRAKE SET, PLACED IN 2ND GEAR (MANUAL TRANSMISSIONS /ENGINE OFF) OR PARK / NEUTRAL (AUTOMATIC TRANSMISSIONS) AND HAVE THE FRONT WHEELS ALIGNED WITH THE LANE STRIPING.

NOTES FOR NIGHTTIME OPERATIONS:

- N1. WORK OCCURRING AFTER SUNSET AND BEFORE SUNRISE WILL BE CONSIDERED NIGHTTIME OPERATIONS.
- N2. ALL SIGNS, STOP/SLOW PADDLES AND RED FLAGS USED TO WARN/ALERT/CONTROL TRAFFIC SHALL BE RETROREFLECTIVE.
- N3. ALL WORKERS INVOLVED SHALL WEAR PROTECTIVE HELMET AND NIGHTTIME APPAREL IN ACCORDANCE WITH §107-05A, HIGH VISIBILITY APPAREL AT ALL TIMES.
- N4. VEHICLES OPERATING ON THE PAVEMENT OF A CLOSED ROADWAY OR TRAVEL LANE SHALL DISPLAY ROTATING AMBER BEACONS OR FLASHING RED BEACONS AT ALL TIMES.
- N5. LEVEL I ILLUMINATION SHALL BE PROVIDED NEAR THE BEGINNING OF LANE CLOSURE TAPERS AND AT ROAD CLOSURES, INCLUDING THE SETUP AND REMOVAL OF THE CLOSURE TAPERS.
- N6. LEVEL II ILLUMINATION SHALL BE PROVIDED FOR FLAGGING STATIONS, ASPHALT PAVING, MILLING, AND CONCRETE PLACEMENT AND/OR REMOVAL OPERATIONS, INCLUDING BRIDGE DECKS, 50 FEET AHEAD OF AND 100 FEET BEHIND A PAVING OR MILLING MACHINE.
- N7. LEVEL III ILLUMINATION SHALL BE PROVIDED FOR PAVEMENT OR STRUCTURAL CRACK FILLING, JOINT REPAIR, PAVEMENT PATCHING AND REPAIRS, INSTALLATION OF SIGNAL EQUIPMENT OR OTHER ELECTRICAL/MECHANICAL, AND OTHER TASKS INVOLVING FINE DETAILS OR INTRICATE PARTS AND EQUIPMENT.
- N8. ALL LIGHTING SHALL BE DESIGNED, INSTALLED, AND OPERATED TO AVOID GLARE THAT AFFECTS TRAFFIC ON THE ROADWAY OR THAT CAUSES ANNOYANCE OR DISCOMFORT FOR RESIDENCES ADJOINING THE ROADWAY.
- N9. PRIOR TO THE START OF NIGHTTIME OPERATIONS, A WRITTEN NIGHTTIME OPERATIONS AND LIGHTING PLAN IS REQUIRED FOR APPROVAL FROM THE DOT ENGINEER.
- N10. SEE STANDARD SPECIFICATIONS §619 FOR ADDITIONAL REQUIREMENTS AND CONSIDERATIONS.
- N11. FLAGGERS SHALL USE A FLASHLIGHT WITH RED GLOW CONE/RED LED BATON FOR FLAGGING IN NON-ILLUMINATED FLAGGER STATIONS DURING NIGHTTIME OPERATIONS.

REFER TO SHEET 2 OF 2 FOR ALL TABLES



NEW YORK
STATE OF
OPPORTUNITY.

Department of
Transportation

U.S. CUSTOMARY STANDARD SHEET

WORK ZONE TRAFFIC CONTROL
TWO-LANE TWO-WAY ROADWAY
SINGLE LANE SHIFT WITH TWO-WAY LEFT
TURN LANE INTERMEDIATE TERM OPERATION
(SHEET 1 OF 2)

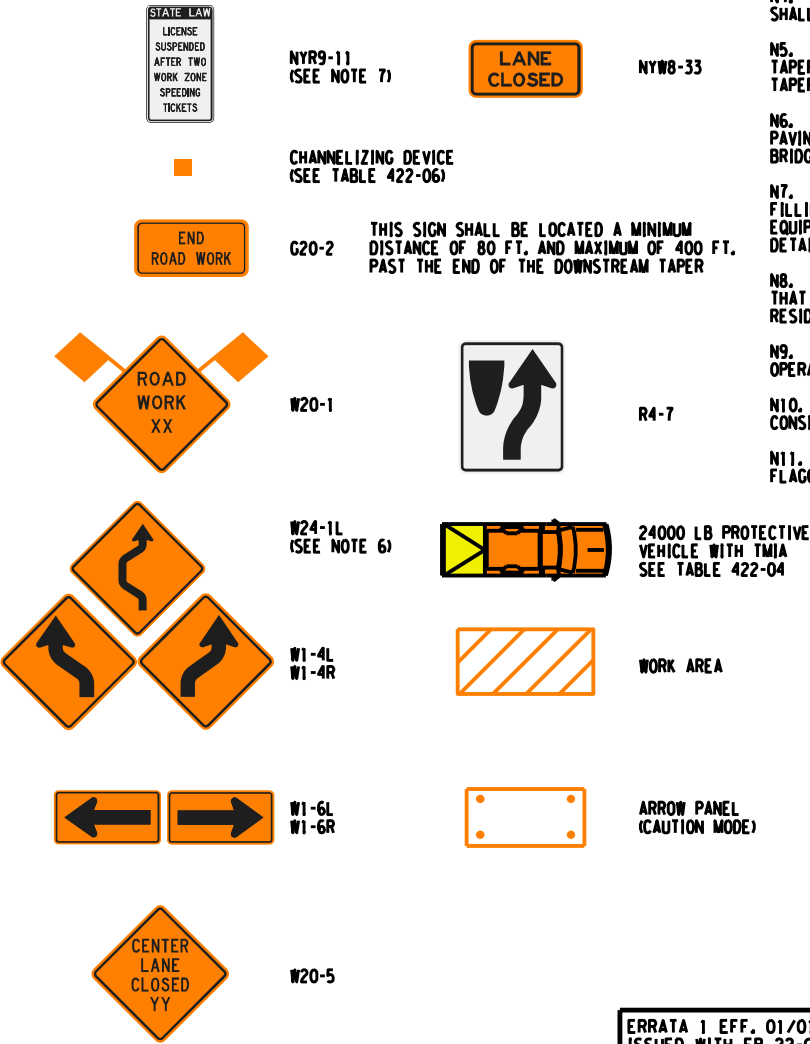
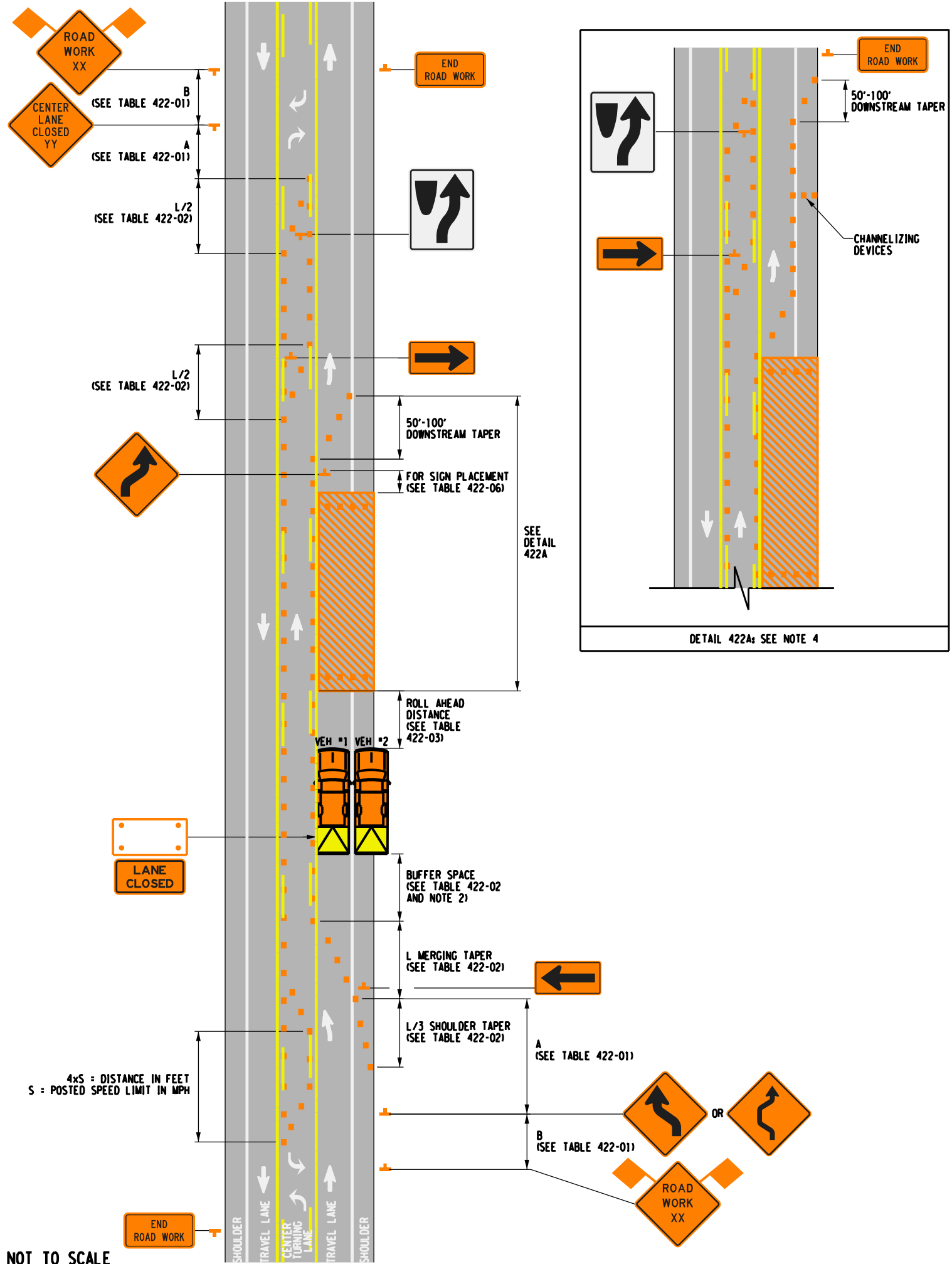
APPROVED DECEMBER 2, 2021

ISSUED UNDER EI 21-028

Robert Limoges
ROBERT LIMOGES, P.E.
DIRECTOR, OTSM

619-422

ERRATA 1 EFF. 01/01/23
ISSUED WITH EB 22-050



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NOT TO SCALE

TABLE 422-01: ADVANCE WARNING SIGN SPACING					
ROAD TYPE	DISTANCE BETWEEN SIGNS			SIGN LEGEND	
	A (FT.)	B (FT.)	C (FT.)	XX	YY
URBAN (≤ 30 MPH*)	100	100	100	AHEAD	AHEAD
URBAN (35-40 MPH*)	200	200	200	AHEAD	AHEAD
URBAN (≥ 45 MPH*)	350	350	350	1000 FT.	AHEAD
RURAL	500	500	500	1500 FT.	1000 FT.
* PRECONSTRUCTION POSTED SPEED LIMIT					

TABLE 422-02: LONGITUDINAL BUFFER SPACE AND TAPER LENGTHS							
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	LONGITUDINAL BUFFER SPACE DISTANCE (FT.)/ * OF SKIP LINES	TAPER LENGTH: L (FT.)/ * OF SKIP LINES/ * OF CHANNELIZING DEVICES			SHOULDER TAPER LENGTH: L/3 (FT.)/ * OF SKIP LINES/ * OF CHANNELIZING DEVICES		
		FOR LANE WIDTH IN FT. (LATERAL SHIFT OF TRAFFIC FLOW PATH)			FOR SHOULDER WIDTH		
		10	11	12	≤ 4 FT.	5 - 7 FT.	≥ 8 FT.
25	155/4	120/3/4	120/3/4	120/3/4	40/1/2	40/1/2	40/1/2
30	200/5	160/4/5	160/4/5	200/5/6	40/1/2	40/1/2	40/1/2
35	250/6	200/5/6	240/6/7	240/6/7	40/1/2	40/1/2	80/2/3
40	305/8	280/7/8	320/8/9	320/8/9	40/1/2	80/2/3	80/2/3
45	360/9	440/11/12	520/13/14	560/14/15	80/2/3	80/2/3	120/3/4
50	425/11	520/13/14	560/14/15	600/15/16	80/2/3	120/3/4	160/4/5
55	495/13	560/14/15	600/15/16	680/17/18	80/2/3	120/3/4	160/4/5


TABLE 422-03: ROLL AHEAD DISTANCE		
ROLL AHEAD DISTANCE (FT.)/* OF SKIP LINES FOR VEHICLES		
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	STATIONARY OPERATION	
	MIN	MAX
≥ 55	120/3	200/5
45 - 50	80/2	160/4
≤ 40	40/1	120/3

TABLE 422-04: PROTECTIVE VEHICLE REQUIREMENTS				
CLOSURE TYPE	ROAD TYPE & SPEED	NON-FREEWAY		
		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH
	EXPOSURE CONDITIONS ¹			
LANE CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	P, TMIA	SEE NOTE 2
	OTHER HAZARDS NO WORKERS EXPOSED	P, TMIA	SEE NOTE 2	SEE NOTE 2
SHOULDER CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	SEE NOTE 2	SEE NOTE 2
	OTHER HAZARDS NO WORKERS EXPOSED	SEE NOTE 2	SEE NOTE 2	SEE NOTE 2
LEGEND				
P: PROTECTIVE VEHICLE REQUIRED FOR EACH CLOSED LANE & EACH CLOSED PAVED SHOULDER 8' OR WIDER, IF THE WORK SPACE MOVES WITHIN THE STATIONARY CLOSURE, THE PROTECTIVE VEHICLE SHALL BE REPOSITIONED ACCORDINGLY				
TMIA: TMIA REQUIRED				
NOTES:				
1. THE EXPOSURE CONDITIONS ASSUME THERE IS NO POSITIVE PROTECTION PRESENT				
2. EITHER A PROTECTIVE VEHICLE OR THE STANDARD BUFFER SPACE SHALL BE PROVIDED				

TABLE 422-05: CHANNELIZING DEVICE APPLICATION FOR INTERMEDIATE-TERM STATIONARY WORK ZONES										
WORK ZONE PROVISIONS	MAXIMUM DEVICE SPACING (CENTER TO CENTER)	MUTCD COMPLIANT CHANNELIZING DEVICE								
INTERMEDIATE-TERM STATIONARY WORK ZONES INVOLVE WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR		DRUMS	STANDARD CONES	TALL CONES	EXTRA TALL CONES	TEMPORARY TUBULAR MARKERS	INTERIM TUBULAR MARKERS	VERTICAL PANELS	OVERSIZED VERTICAL PANELS	TYPE III BARRICADES
SHOULDER/MERGING/ SHIFTING TAPERS	20 FT. •	X							X	
	40 FT.	X							X	
MARKING FOR TRANSVERSE BUMPS ¹	N/A	X ²			X ²				X ²	
TRANSVERSE DEVICE WITHIN CLOSED TRAFFIC LANE AND/OR SHOULDER	800 FT.	X		X	X			X	X	0
REMOVAL OF EXISTING GUIDE RAIL	80 FT.	X		X	X	X		X	X	0
	40 FT.									
NOTES: X= ALLOWED, BLANK = NOT ALLOWED, 0 = OPTIONAL										
1. - A TYPE 1 OBJECT MARKER MAY BE USED IN LIEU OF CHANNELIZING DEVICE.										
2. - CHANNELIZING DEVICES SHALL BE EQUIPPED WITH A FLASHING WARNING LIGHT.										
• SEE NOTE 3 ON SHEET 1 OF 2.										

TABLE 422-06: GUIDELINES FOR ADVANCE PLACEMENT OF WARNING SIGNS														
POSTED OR 85 TH - PERCENTILE SPEED (MPH)	ADVANCE PLACEMENT DISTANCE (FT.) ¹													
	CONDITION A: SPEED REDUCTION AND LANE CHANGING IN HEAVY TRAFFIC ²	CONDITION B: DECELERATION TO THE LISTED ADVISORY SPEED (MPH) FOR THE CONDITION ⁴												
		CONDITION C: NO SPEED REDUCTION NECESSARY ⁵												
		0 ³	5	10	15	20	25	30	35	40	45	50	55	
20	410	115	110	105	90	75	-	-	-	-	-	-	-	
25	515	155	160	150	135	120	95	-	-	-	-	-	-	
30	620	200	205	195	185	165	140	110	-	-	-	-	-	
35	720	250	255	245	235	215	190	160	130	-	-	-	-	
40	825	305	320	310	295	280	255	225	190	150	-	-	-	
45	930	360	380	370	360	340	315	285	255	210	165	-	-	
50	1030	425	455	450	435	415	390	360	330	285	240	185	-	
55	1135	495	530	520	505	490	460	435	400	355	315	255	205	
NOTES:														
1. THE DISTANCES HAVE NOT BEEN MODIFIED TO ACCOUNT FOR SIGN LEGIBILITY.														
2. TYPICAL CONDITIONS ARE LOCATIONS WHERE THE ROAD USER MUST USE EXTRA TIME TO ADJUST SPEED AND CHANGE LANES IN HEAVY TRAFFIC BECAUSE OF A COMPLEX DRIVING SITUATION. TYPICAL SIGNS ARE MERGE AND RIGHT LAND ENDS. THE DISTANCES ARE TAKEN FROM THE 2004 AASHTO POLICY, EXHIBIT 3-3, DECISION SIGHT DISTANCE, AVOIDANCE MANEUVER E.														
3. TYPICAL CONDITION IS THE WARNING OF A POTENTIAL STOP SITUATION. TYPICAL SIGNS ARE STOP AHEAD, YIELD AHEAD, SIGNAL AHEAD, AND INTERSECTION WARNING SIGNS. THE DISTANCES ARE TAKEN FROM THE 2004 AASHTO POLICY, STOPPING SIGHT DISTANCE, EXHIBIT 3-1.														
4. TYPICAL CONDITIONS ARE LOCATIONS WHERE THE ROAD USER MUST DECREASE SPEED TO MANEUVER THROUGH THE WARNED CONDITION. TYPICAL SIGNS ARE TURN, CURVE, REVERSE TURN, OR REVERSE CURVE. THE DISTANCES ARE DETERMINED BY PROVIDING A 2.5 SECOND PRT AND A VEHICLE DECELERATION RATE OF 10 FT/SECOND. ²														
5. TYPICAL SIGNS ARE ADDED LANE, ROAD NARROWS, DIVIDED HIGHWAY, AND CONDITION B SIGNS DISPLAYED WITHOUT AN ADVISORY SPEED.														

TABLE 422-07: REQUIRED SIGN SIZES*		
SIGN	NON-FREEWAY	FREEWAY
NYR9-11	24x42	48x84
W1-4L/W1-4R	36x36	48x48
W1-6L/W1-6R	48x24	60x30
W9-3	36x36	48x48
W20-1	36x36	48x48
W20-4	36x36	48x48
W20-5	36x36	48x48
W24-1L	36x36	48x48
R4-7	24x30	36x48
NYW8-33	48x24	48x24
G20-2	36x18	48x24
WARNING FLAG	18x18	18x18
*FREEWAY SIZES MAY BE USED ON NON-FREEWAY, IF SPACE CONSTRAINTS DO NOT EXIST.		



NEW YORK
STATE OF
OPPORTUNITY.

**Department of
Transportation**

U.S. CUSTOMARY STANDARD SHEET

WORK ZONE TRAFFIC CONTROL
TWO-LANE TWO-WAY ROADWAY
SINGLE LANE SHIFT WITH TWO-WAY LEFT
TURN LANE INTERMEDIATE TERM OPERATION
(SHEET 2 OF 2)

APPROVED APRIL 8, 2022

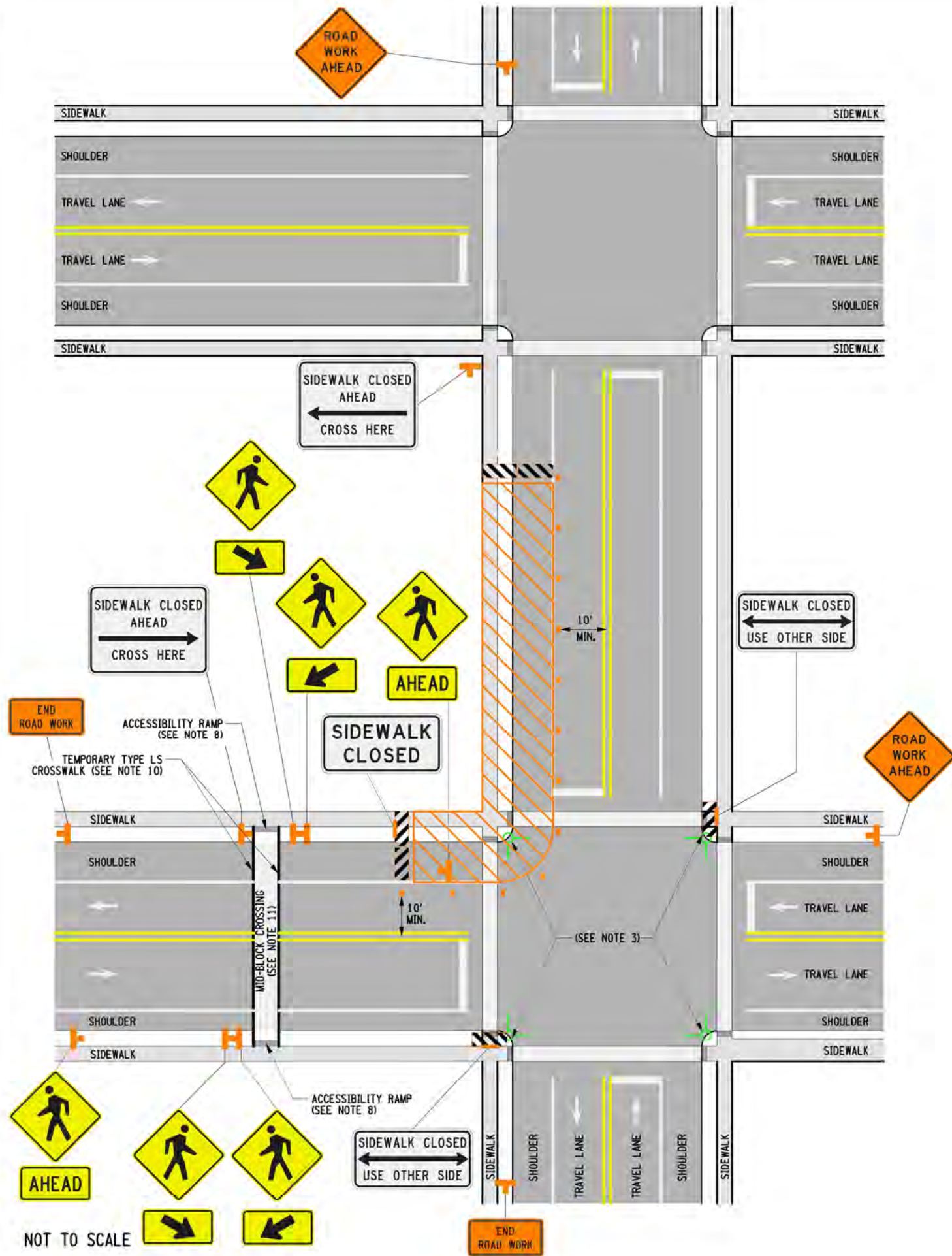
ISSUED UNDER EI 22-008

ROBERT LIMOGES, P.E.
DIRECTOR, OTSM

619-422

USC May 01, 2023

FILE NAME = 619-520-1.dgn
DATE/TIME = 06-DEC-2021 14:35
USER = wahren



END
ROAD WORK

G20-2 THIS SIGN SHALL BE LOCATED A MINIMUM DISTANCE OF 80 FT. AND MAXIMUM OF 400 FT. PAST THE END OF THE DOWNSTREAM TAPER

AHEAD

W16-9P

ROAD
WORK
AHEAD

W20-1

W16-7PR



W16-7PL

SIDEWALK CLOSED
AHEAD
CROSS HERE

R9-11L

W11-2L



W11-2L

SIDEWALK
CLOSED

R9-9



R8-3
(SEE NOTE 2)

SIDEWALK CLOSED
AHEAD
CROSS HERE

R9-11R

SIDEWALK CLOSED
USE OTHER SIDE

R9-10



WORK AREA



CHANNELIZING DEVICE
(SEE TABLE 520-02)

TYPE III
BARRICADE

REFER TO SHEET 2 OF 2 FOR ALL TABLES

	Department of Transportation
U.S. CUSTOMARY STANDARD SHEET	
WORK ZONE TRAFFIC CONTROL TWO-LANE TWO-WAY ROADWAY CROSSWALK CLOSURE AND PEDESTRIAN DETOUR LONG TERM OPERATION (SHEET 1 OF 2)	
APPROVED DECEMBER 2, 2021 <i>Robert Limoges</i> ROBERT LIMOGES, P.E. DIRECTOR, OTSM	ISSUED UNDER EI 21-028 619-520

TABLE 520-01: REQUIRED SIGN SIZES*		
SIGN	NON-FREEWAY	FREEWAY
G20-2	36x18	48x24
R8-3	24x24	48x48
R9-9**	24x12	24x12
R9-10**	24x12	24x12
R9-11L/R9-11R**	24x18	24x18
W11-2L/W11-2R	48x30	48x30
W16-9P**	24x12	30x18
W16-7PL/W16-7PR**	24x12	30x18
*FREEWAY SIZES MAY BE USED ON NON-FREEWAY, IF SPACE CONSTRAINTS DO NOT EXIST. **SIGNS NOT FOR FREEWAY USE.		

TABLE 520-02: CHANNELIZING DEVICE APPLICATION FOR LONG-TERM STATIONARY WORK ZONES										
WORK ZONE PROVISIONS	MAXIMUM DEVICE SPACING (CENTER TO CENTER)	MUTCD COMPLIANT CHANNELIZING DEVICE								
		DRUMS	STANDARD CONES	TALL CONES	EXTRA TALL CONES	TEMPORARY TUBULAR MARKERS	INTERIM TUBULAR MARKERS	VERTICAL PANELS	OVERSIZED VERTICAL PANELS	TYPE III BARRICADES
	20 FT.	X							X	
NOTES: X= ALLOWED, BLANK = NOT ALLOWED										

TABLE 520-03: GUIDELINES FOR ADVANCE PLACEMENT OF WARNING SIGNS														
POSTED OR 85 TH PERCENTILE SPEED (MPH)	ADVANCE PLACEMENT DISTANCE (FT.) ¹													
	CONDITION A: SPEED REDUCTION AND LANE CHANGING IN HEAVY TRAFFIC ²	CONDITION B: DECELERATION TO THE LISTED ADVISORY SPEED (MPH) FOR THE CONDITION ⁴												
		CONDITION C: NO SPEED REDUCTION NECESSARY ⁵	0 ³	5	10	15	20	25	30	35	40	45	50	55
20	410	115	110	105	90	75	-	-	-	-	-	-	-	
25	515	155	160	150	135	120	95	-	-	-	-	-	-	
30	620	200	205	195	185	165	140	110	-	-	-	-	-	
35	720	250	255	245	235	215	190	160	130	-	-	-	-	
40	825	305	320	310	295	280	255	225	190	150	-	-	-	
45	930	360	380	370	360	340	315	285	255	210	165	-	-	
50	1030	425	455	450	435	415	390	360	330	285	240	185	-	
55	1135	495	530	520	505	490	460	435	400	355	315	255	205	
NOTES: 1. THE DISTANCES HAVE NOT BEEN MODIFIED TO ACCOUNT FOR SIGN LEGIBILITY. 2. TYPICAL CONDITIONS ARE LOCATIONS WHERE THE ROAD USER MUST USE EXTRA TIME TO ADJUST SPEED AND CHANGE LANES IN HEAVY TRAFFIC BECAUSE OF A COMPLEX DRIVING SITUATION. TYPICAL SIGNS ARE MERGE AND RIGHT LAND ENDS. THE DISTANCES ARE TAKEN FROM THE 2004 AASHTO POLICY, EXHIBIT 3-3, DECISION SIGHT DISTANCE, AVOIDANCE MANEUVER E. 3. TYPICAL CONDITION IS THE WARNING OF A POTENTIAL STOP SITUATION. TYPICAL SIGNS ARE STOP AHEAD, YIELD AHEAD, SIGNAL AHEAD, AND INTERSECTION WARNING SIGNS. 4. TYPICAL CONDITIONS ARE LOCATIONS WHERE THE ROAD USER MUST DECREASE SPEED TO MANEUVER THROUGH THE WARNED CONDITION. TYPICAL SIGNS ARE TURN, CURVE, REVERSE TURN, OR REVERSE CURVE. THE DISTANCES ARE DETERMINED BY PROVIDING A 2.5 SECOND PRT AND A VEHICLE DECELERATION RATE OF 10 FT./SECOND. ² 5. TYPICAL SIGNS ARE ADDED LANE, ROAD NARROWS, DIVIDED HIGHWAY, AND CONDITION B SIGNS DISPLAYED WITHOUT AN ADVISORY SPEED.														

NOTES:

- LONG-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN 3 CONSECUTIVE DAYS. THIS SETUP CAN ALSO BE USED FOR INTERMEDIATE TERM WORK.
- PARKING ALONG THE CURB SHALL BE PROHIBITED FOR AT LEAST 50' IN ADVANCE OF THE MID-BLOCK CROSSWALK. IF THE SHOULDER IS GREATER THAN OR EQUAL TO 8 FOOT, AN R8-3 SIGN IS RECOMMENDED TO PREVENT PARKING ON SHOULDER.
- PEDESTRIAN TRAFFIC SIGNAL DISPLAYS CONTROLLING CLOSED CROSSWALKS SHOULD BE COVERED OR DEACTIVATED.
- ONLY THE WORK ZONE TRAFFIC CONTROL DEVICES RELATED TO PEDESTRIANS ARE SHOWN. OTHER DEVICES, SUCH AS LANE/SHOULDER CLOSURE SIGNING, ROAD NARROWS SIGNS (W5-4), OR NO PARKING SIGNS MAY BE USED TO CONTROL VEHICULAR TRAFFIC.
- ANY FEATURES CONSTRUCTED AS PART OF A PEDESTRIAN DETOUR MUST BE ADA COMPLIANT AND FOLLOW THE 2011 PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG). TEMPORARY RAMPS AND OTHER TEMPORARRY PEDESTRIAN FACILITIES SHALL BE PAID FOR UNDER THE APPROPRIATE ITEM FOR ASPHALT SIDEWALKS.
- PROWAG SECTION R205 AND MUTCD PART 6 SHALL APPLY TO ALL TEMPORARY ALTERNATIVE PEDESTRIAN PATHWAYS.
- TYPE III BARRICADES SHALL BE THE FULL WIDTH OF THE PATH BEING CLOSED.
- THE GRADE (RUNNING SLOPE), AND CROSS SLOPE OF A TEMPORARY CURB RAMP FOR DESIGN AND LAYOUT SHALL BE IN ACCORDANCE WITH STANDARD SHEET 608-01.
- ACCESS TO BUSINESS ENTRANCES AND TRANSIT STOPS WILL NEED TO BE MAINTAINED. IF THE PEDESTRIAN FACILITY CURRENTLY HAS A TRANSIT STOP THAT WILL BE AFFECTED BY THE WORK ZONE, PRACTITIONERS SHOULD CONSULT THE TRANSIT AUTHORITIES THAT SERVICE THE AFFECTED STOP TO DETERMINE HOW TO ACCOMMODATE THE STOP.
- FOR LONG-TERM STATIONARY WORK, THE DOUBLE YELLOW CENTERLINE AND/OR LANE LINES SHOULD BE REMOVED BETWEEN THE TEMPORARY CROSSWALK LINES.
- ALL MID BLOCK CROSSWALKS SHALL BE APPROVED BY THE DOT REGIONAL DIRECTOR OR HIS/HER DESIGNEE.

NOTES ON NIGHTTIME WORK:

- WORK OCCURRING AFTER SUNSET AND BEFORE SUNRISE WILL BE CONSIDERED NIGHTTIME OPERATIONS.
- ALL SIGNS, STOP/SLOW PADDLES AND RED FLAGS USED TO WARN/ALERT/CONTROL TRAFFIC SHALL BE RETROREFLECTIVE.
- ALL WORKERS INVOLVED SHALL WEAR PROTECTIVE HELMETS AND NIGHTTIME APPAREL IN ACCORDANCE WITH §107-05A. HIGH VISIBILITY APPAREL AT ALL TIMES.
- VEHICLES OPERATING ON THE PAVEMENT OF A CLOSED ROADWAY OR TRAVEL LANE SHALL DISPLAY ROTATING AMBER BEACONS OR FLASHING LED BEACONS AT ALL TIMES.
- LEVEL I ILLUMINATION SHALL BE PROVIDED NEAR THE BEGINNING OF LANE CLOSURE TAPERS AND AT ROAD CLOSURES, INCLUDING THE SETUP AND REMOVAL OF THE CLOSURE TAPERS.
- LEVEL II ILLUMINATION SHALL BE PROVIDED FOR FLAGGING STATIONS, ASPHALT PAVING, MILLING, AND CONCRETE PLACEMENT AND/OR REMOVAL OPERATIONS, INCLUDING BRIDGE DECKS, 50 FEET AHEAD OF AND 100 FEET BEHIND A PAVING OR MILLING MACHINE.
- LEVEL III ILLUMINATION SHALL BE PROVIDED FOR PAVEMENT OR STRUCTURAL CRACK FILLING, JOINT REPAIR, PAVEMENT PATCHING AND REPAIRS, INSTALLATION OF SIGNAL EQUIPMENT OR OTHER ELECTRICAL/MECHANICAL EQUIPMENT, AND OTHER TASKS INVOLVING FINE DETAILS OR INTRICATE PARTS AND EQUIPMENT.
- ALL LIGHTING SHALL BE DESIGNED, INSTALLED, AND OPERATED TO AVOID CLARE THAT AFFECTS TRAFFIC ON THE ROADWAY OR THAT CAUSES ANNOYANCE OR DISCOMFORT FOR RESIDENCES ADJOINING THE ROADWAY.
- PRIOR TO THE START OF NIGHTTIME OPERATIONS, A WRITTEN NIGHTTIME OPERATIONS AND LIGHTING PLAN IS REQUIRED FOR APPROVAL FROM THE DOT ENGINEER.
- AT NIGHT, EACH TYPE III CONSTRUCTION BARRICADE USED TO CLOSE A ROADWAY, A SEGMENT OF A ROADWAY OR A SIDEWALK SHALL BE EQUIPPED WITH ONE FLASHING WARNING LIGHT.
- SEE STANDARD SPECIFICATIONS §619 FOR ADDITIONAL REQUIREMENTS AND CONSIDERATIONS.



Department of
Transportation

U.S. CUSTOMARY STANDARD SHEET

WORK ZONE TRAFFIC CONTROL
TWO-LANE TWO-WAY ROADWAY
CROSSWALK CLOSURE AND PEDESTRIAN DETOUR
LONG TERM OPERATION
(SHEET 2 OF 2)

APPROVED APRIL 8, 2022

ISSUED UNDER EI 22-008

Robert Limoges

619-520

ROBERT LIMOGES, P.E.
DIRECTOR, OTSM

ERRATA 1 EFF. 05/01/23
ISSUED WITH EB 23-007

SPECIAL NOTES LISTING

SPECIAL NOTES LIST

- CONR 9K Supplemental Information For Bidders
- Controlling Exposure to Diesel Exhaust
- Coordination With Utilities
- Dust Control
- Electronic Data
- Insurance Supplement
- No Utility Involvement
- NYSDOT Work Permits
- Pedestrian and Bicycle Traffic
- Right-of-Way Note
- Special Specification Item Numbers
- Survey Work for Sidewalks and Curb Ramps
- Temporary Lane/Shoulder Closure Restrictions for Major Holidays
- Ultra Low Sulfur Diesel Fuel

SUPPLEMENTAL INFORMATION AVAILABLE TO BIDDERS

The information checked in the "Digital" column on this form is available at the Contract Documents tab within the Construction Contracting section of the [Business Center](#) on the Department's web site. The information checked in the "Inspection Only" column on this form is available at the Regional Office having jurisdiction for this project, as identified in the advertisement for bids, for inspection and review prior to the letting date. The bidder's signature on this proposal certifies that they have made themselves aware of the availability of the information indicated below:

THERE IS NO SUPPLEMENTAL INFORMATION AVAILABLE FOR THIS CONTRACT: ☐

INFORMATION	Digital ¹	Inspection Only
1. Unsealed Layered or 3D PDF Files	<input type="checkbox"/>	
2. CADD Information		
a. MicroStation DGN	<input type="checkbox"/>	
b. InRoads DTM and XML format	<input type="checkbox"/>	
c. InRoads ALG and XML format	<input type="checkbox"/>	
3. Cross Sections in ADOBE PDF format	<input type="checkbox"/>	
4. Quantity Work-ups ²	<input type="checkbox"/>	
5. Record Plans	<input type="checkbox"/>	<input type="checkbox"/>
6. Rock Cores (available for inspection only)		<input type="checkbox"/>
7. Sign Face Layouts in ADOBE PDF format	<input type="checkbox"/>	
8. Stormwater Pollution Prevention Plan (SWPPP)	<input type="checkbox"/>	<input type="checkbox"/>
9. Subsurface Information		
a. Subsurface Exploration Logs	<input type="checkbox"/>	<input type="checkbox"/>
b. Undisturbed Sample Logs	<input type="checkbox"/>	<input type="checkbox"/>
c. Laboratory Test Data from Soil Samples	<input type="checkbox"/>	<input type="checkbox"/>
d. Tabulated Results of Probing	<input type="checkbox"/>	<input type="checkbox"/>
e. Tabulated Depth to Bedrock	<input type="checkbox"/>	<input type="checkbox"/>
f. Rock Core Evaluation Logs	<input type="checkbox"/>	<input type="checkbox"/>
g. Compression Test Data from Rock Samples	<input type="checkbox"/>	<input type="checkbox"/>
h. Rock Outcrop Maps	<input type="checkbox"/>	<input type="checkbox"/>
i. Granular Materials Resource Survey Reports	<input type="checkbox"/>	<input type="checkbox"/>
j. Terrain Reconnaissance Reports	<input type="checkbox"/>	<input type="checkbox"/>
10. Subsurface Information - Other Information		
a. Subsurface information from outside sources	<input type="checkbox"/>	<input type="checkbox"/>
b. Source Information - Granular Material and aggregates	<input type="checkbox"/>	<input type="checkbox"/>
c. Special Subsurface Reports	<input type="checkbox"/>	<input type="checkbox"/>
11. Anticipated Construction Schedule	<input type="checkbox"/>	<input type="checkbox"/>
12. Asbestos Information		
a. Asbestos Blanket Variances	<input type="checkbox"/>	<input type="checkbox"/>
b. Asbestos Report	<input type="checkbox"/>	<input type="checkbox"/>
13. Special Reports or Other Information:		
a. Permits	<input type="checkbox"/>	<input type="checkbox"/>
b. Design Approval Document	<input type="checkbox"/>	<input type="checkbox"/>
c. Survey Control Report	<input type="checkbox"/>	<input type="checkbox"/>
d. Wetland Compensation Report	<input type="checkbox"/>	<input type="checkbox"/>
14. AutoCAD C3D (see Special Note for list of files)	<input checked="" type="checkbox"/>	<input type="checkbox"/>
	<input type="checkbox"/>	<input type="checkbox"/>

¹ – All digital material is provided in ADOBE (PDF) format, unless noted above.

² – Required for all projects.

SPECIAL NOTE

CONTROLLING EXPOSURE TO DIESEL EXHAUST

The Contractor shall exercise measures to protect “Sensitive Receptors” from the impacts of diesel exhaust fumes. Sensitive Receptors include, but are not limited to: hospitals, schools, daycare facilities, building fresh air or ventilation intakes, elderly housing or convalescent facilities. The Contractor shall ensure that diesel powered engines are located away from building air conditioners and windows.

The goal is to minimize exposure of Sensitive Receptors in close proximity to diesel exhaust, in terms of both concentration and time. In general, close proximity is defined as within 50 feet of a Sensitive Receptor. Mitigation techniques include positioning stationary equipment exhausts greater than 50 feet from Sensitive Receptors, extension of equipment exhausts through the use of flexible tubing; protecting building air intakes; and the use of moving operations.

Idling time for diesel powered equipment shall be limited to three consecutive minutes for delivery and dump trucks and all other diesel powered equipment except as follows:

- When a “mobile source” (vehicle) is forced to remain motionless because of traffic conditions or mechanical difficulties over which the operator has no control.
- When it is necessary to operate a loading, unloading or processing device.
- When the outdoor temperature is less than - 3°C (27°F).
- When the “mobile source” is being repaired.

Arrow panels and portable variable message signs shall be solar powered wherever possible or practical.

Whenever possible and practicable, the Contractor shall establish staging areas for diesel powered vehicles waiting to load or unload materials at the work site. Such areas shall be located where diesel emissions have the least impact on Sensitive Receptors and the general public.

SPECIAL NOTE

DUST CONTROL

The Contractor shall minimize dust from disturbed soil surfaces or other materials that can cause off-site damage, health hazards and traffic safety problems. Dusty conditions resulting from the Contractor's operations shall be corrected at no additional cost to the Village. Buffer areas of vegetation should be left where practical. Water quality shall be considered when selecting materials for dust control. An approved dust palliative may be used in conformance with applicable conditions placed on its use. A list of acceptable dust palliatives is available at: <https://www.dot.ny.gov/divisions/engineering/technical-services/geotechnical-engineering-bureau/dust-palliatives>.

For areas not subject to traffic, products and materials may be applied or placed on soil surfaces to prevent airborne migration of soil particles, including:

- Vegetative Cover –provides the most practical method of dust control.
- Mulch (including rolled erosion control products) –provides a fast, effective method of dust control.
- Spray Adhesives –Generally composed of polymers in a liquid or solid form mixed with water to form an emulsion that is sprayed on the soil surface. The mixing ratios and application rates will be in accordance with the manufacturer's recommendations for the specific soils on the site. Adhesives shall not be applied to wet soils or if there is a probability of precipitation within 48 hours.

For areas subject to traffic (traveling public or construction traffic) products and materials may be applied or placed on soil surfaces to prevent airborne migration of soil particles, including:

- Water Sprinkling –The site may be sprayed with water until the surface is wet. This is especially effective on haul roads and access routes.
- Polymer Additives –Polymers shall be mixed with water and applied to the driving surface using mixing ratios and application rates in accordance with the manufacturer's recommendations. No application of the polymer will be made if there is a probability of precipitation within 48 hours of its proposed use. Any polymers must be used in accordance with the NYSDEC issued "Conditions for Use" and "Application Instructions." This information can be obtained from the NYSDEC website.
- Barriers –Woven geotextiles or stone can be placed on the driving surface to effectively reduce dust throw and particle migration on haul roads.
- Windbreak –A silt fence or similar barrier can control air currents at horizontal intervals equal to ten times the barrier height. Preserve existing vegetation that acts as a wind barrier as much as practical.
- Wheel Washing –Mechanical or manual wet-method cleaning of on-road construction vehicle tires prior to leaving site.

SPECIAL NOTE

ELECTRONIC DATA

The following electronic data files, covering the project work, are available to the Contractor as supplemental information for this project. Note that all files are in AutoCAD format. No supplemental formats will be provided.

Design Files:

8762.48_CPH_dat_rwy.dwg	Design File
8762.48_CPH_dat_Wetland.dwg	Wetland File
8762.48_fea_env.dwg	Erosion Sediment Control design file
8762.48_fea_grn.dwg	Green Infrastructure design file
8762.48_fea_lnd.dwg	Landscaping design file
8762.48_fea_drn.dwg	Drainage design file

Surface Files:

Existing Conditions

8656topo7-28-20.dwg	Original ground surface
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Proposed Conditions

8762.48_fea_rwy_surf_FINAL.dwg	final design surface
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Alignment Files:

8762.48_fea_rwy_driveways_alg_C3D.dwg	mainline alignments and profiles
8762.48_fea_rwy_sidestreets_alg_C3D.dwg	side street alignments and profiles
8762.48_fea_rwy_driveways_alg_C3D.dwg	driveway alignments and profiles

LIST OF ADDITIONAL INSURED PARTIES

In accordance with Standard Specifications §107-06A.4 applicable insurance policies shall be endorsed to provide coverage to:

- The State of New York / New York State Department of Transportation
- Any municipality in which the work is being performed
- Any public benefit corporation, railroad, or public utility whose property or facilities are affected by the work
- Any consultants working for or on the project
- Agents or employees of the above listed parties

As any new locations of work are defined or added to the Contract, the Contractor shall extend coverage to any new parties that warrant coverage as per §107-06A.4. Proof of coverage for the new additional insured parties shall be provided to the Department.

Coverage shall be extended to the following known additional insured parties:

- County of Ulster
- Village of New Paltz
- Verizon
- Central Hudson Gas & Electric
- Spectrum
- Alta Planning + Design, Inc.
- H+Z Engineering + Landscape Architecture, D.P.C.
- Control Point Associates, Inc.

SPECIAL NOTE
COORDINATION WITH THE UTILITY SCHEDULE - NO ANTICIPATED INVOLVEMENT

Utility facility adjustments and/or relocations are not anticipated for this project. If the Department determines that utility facility adjustments and/or relocations will be necessary, the provisions of Section 102-09 Other Contracts, Coordination and Access will apply.

Any such adjustments and/or relocations will be performed by the Utility owners and/or the state contractor upon direction by the State Engineer-In-Charge.

Suitable time frames for these additions shall be coordinated between the State, the Contractor, and the affected Utility.

The contractor is governed by and must adhere to the provisions of 16 NYCRR Part 753 (Protection of Underground Facilities).

SPECIAL NOTE

NYSDOT HIGHWAY WORK PERMITS

The contractor shall be responsible for obtaining the following NYSDOT Work Permits and providing them to the NYSDOT Permit Engineer:

1. [PERM 33 - Non-Utility Work Permit](#) (4 Original Copies. The Village of New Paltz will be co-applicant)
2. Insurance Forms
 - a. ACORD 25 – CGL – Min. 1,000,000 per claim/occurrence
 - b. ACORD 855
 - c. Workers Comp – C105.2, U-26.3, SI-12 or CE-200 exempt
 - d. Disability – DB120.1, DB-155 or CE-200 exempt
3. [PERM 44 – Surety Bond](#) in the amount of \$50,000

Certificate holder on all forms to be:

NYSDOT
11 Quarry Street
Kingston, NY 12401

The contractor is responsible for all permit paperwork, bond, and insurance fees associated with the above permit forms. The contractor shall provide a surety bond per PERM 44 in the amount specified by the NYSDOT Regional Permit Engineer. The submission and acceptance of these forms is required prior to the contractor beginning work on the project.

For additional information, refer to:

<https://www.dot.ny.gov/divisions/operating/oom/transportation-systems/traffic-operations-section/highway-permits>

No additional payment will be made to the Contractor for work efforts or fees related to the above forms/permits.

SPECIAL NOTE

PEDESTRIAN AND BICYCLE TRAFFIC

The Contractor's attention is called to the fact that pedestrian and bicycle traffic is to be maintained throughout or around the project for the duration of construction. Material, equipment or other such barriers shall not be placed or parked so as to obstruct pedestrian / bicycle traffic or present a safety hazard to the non-motorized public.

All necessary labor, material and equipment necessary to maintain pedestrian and bicycle traffic shall be included in the bid for Item 619.01 – Work Zone Traffic Control.

SPECIAL NOTE

PEDESTRIAN FACILITIES IN THE PUBLIC RIGHT-OF-WAY

The Contractor's attention is called to the fact that Engineering Directive 15-004 was issued by the New York State Department of Transportation, effective immediately, to ensure that newly constructed pedestrian facilities are compliant with the American with Disabilities Act (ADA).

The values shown on the table "Critical Elements for the Design, Layout and Acceptance of Pedestrian Facilities" (available on the NYSDOT Highway Design Manual Chapter 18 internet page: <https://www.dot.ny.gov/divisions/engineering/design/dgab/hdm/chapter-18>) shall be used during construction to ensure that pedestrian facilities in the public right-of-way (ROW) are ADA compliant. In addition, the ADA Reporting table is required to be filled out prior to completion and final payment of the project. This is a requirement by NYSDOT and will not be waived.

The Contractor will be responsible for ensuring that new and reconstructed facilities meet the requirements listed. No additional payments will be made for re-work on pedestrian facilities that fail to meet these requirements.

During construction, if a pedestrian facility cannot practicably be made compliant due to unforeseen existing conditions, a justification in accordance with the Highway Design Manual (HDM) Chapter 2, Exhibit 2-15a is required. The Contractor will coordinate this request through the EIC and NYS Department staff for approval.

ADA Reporting Table - Curb Ramps
 HWY Work PERMIT [# TBD]
 Village of New Paltz, Ulster County

Curb Ramps									
Location					Ramp Type ²	New, Replacement, or Existing to remain ³	NSFJ ⁴ (Yes or No)	Notes	Built to ADA Standards (Yes or No - if No, note nonstandard feature) ⁵
Roadway	Station	Side	Coordinates ¹						
NY RT 32 (SH 572)	N/A	W	41.753045	-74.084981	11	New	No	NW corner Henry W Dubois Dr and NY RT 32 / SH 572 N Chestnut Street	
NY RT 32 (SH 572)	N/A	E	41.753006	-74.084792	5	Replacement	No	NE corner Henry W Dubois Dr and NY RT 32 / SH 572 N Chestnut Street	

- Notes:
1. Coordinates are Northings and Westings as used in Google Earth, in Decimal Degrees.
 2. Ramp Type as per Standard Sheet 608-01 (sheets 5 thru 7), if modified or non standard identify how in Notes column.
 3. Existing curb ramps to remain need to be identified for inclusion in the ADA Transition Plan.
 4. Nonstandard feature Justification Form for Pedestrian Facilities (Exhibit 2-15a).
 5. EIC to verify if curb ramps have been built to ADA standards.

SPECIAL NOTE

RIGHT-OF-WAY NOTE

- A. ALL WORK TO BE PERFORMED UNDER THIS CONTRACT WILL BE WITHIN THE PUBLIC RIGHT-OF-WAY (ROW) IN ACCORDANCE WITH SECTION 105-15 OF THE STANDARD SPECIFICATIONS. THE CONTRACTOR IS TO ASSURE HIMSELF THAT ALL WORK IS BEING PERFORMED WITHIN THE ROW, INCLUDING BUT NOT LIMITED TO VEHICLE ACCESS; STORAGE OF EQUIPMENT, MATERIALS, DEBRIS AND WASTE; LANDSCAPING; VEGETATION REMOVAL AND MANAGEMENT; GRADING, SEEDING AND THE INSTALLATION OF TURF; AND THE INSTALLATION OF ANY FENCES OR PROTECTIVE BARRIER.
- B. IF CONTRACTOR IS UNABLE TO IDENTIFY THE LIMITS OF THE RIGHTS-OF-WAY WHEN THE CONTRACT CALLS FOR WORK IN THOSE VICINITIES, THE CONTRACTOR MUST CONTACT THE PROJECT ENGINEER FOR DEFINITIVE BOUNDARY DETERMINATIONS BEFORE ANY WORK MAY BE INITIATED AT THOSE LOCATIONS (STANDARD SPECIFICATIONS SECTIONS 105-10 AND 625).
- C. IN ACCORDANCE WITH SECTION 107-13 OF THE STANDARD SPECIFICATIONS, RELEASES FOR ANY NON-ESSENTIAL CONTRACT WORK OUTSIDE OF THE EXISTING RIGHTS-OF-WAY, INCLUDING PLANTINGS, LANDSCAPING OR DRIVEWAY ENHANCEMENT, WILL BE PROVIDED BY THE PROJECT ENGINEER AND IN NO INSTANCE ARE TO BE SECURED BY THE CONTRACTOR. THE CONTRACTOR SHALL NOT INVADE UPON PRIVATE PROPERTIES, LANDS OR BUILDINGS OUTSIDE OF THE RIGHTS-OF-WAY FOR ANY REASON WITHOUT FIRST SECURING WRITTEN PERMISSION FROM THE PROPERTY OWNER (STANDARD SPECIFICATIONS SECTIONS 105-15, 107-13).
- D. THE CONTRACTOR WILL BE HELD LIABLE FOR ANY DAMAGES DONE. ANY SUCH INJURIES OR DAMAGES SHALL BE SATISFACTORILY REPAIRED OR ITEMS REPLACED AT THE CONTRACTOR'S EXPENSE (STANDARD SPECIFICATIONS SECTION 107-08 AND 107-13).

SPECIAL NOTE

SPECIAL SPECIFICATION ITEM NUMBERS

The Contractor's attention is directed to the special specification pay item formats used in this contract. Special specification pay items may be presented in two different formats:

- Format 1. Pay items appearing in the special specification will have five digits to the left of the decimal point and up to six digits to the right of the decimal point. The two left-most digits represent the origin of the specification. Reference Standard Specification §101-02 Specifications.
- Format 2. Pay items appearing in the special specification will have three digits to the left of the decimal point and up to eight digits to the right of the decimal. The seventh and eight digits to the right of the decimal will represent the origin of the specification.

Where items in this contract appear in multiple formats, one format shall be equated to the other format as illustrated below:

<u>Format 1</u>	<u>Format 2 *</u>	<u>Format 3</u>
XXXXXX.XX	XXX.XXbbbbXX	XXX.XX0000XX
XXXXXX.XXXX	XXX.XXXXbbXX	XXX.XXXX00XX
XXXXXX.XXXXXX	XXX.XXXXXXXX	XXX.XXXXXXXX

* "b" represents a blank space which will appear when all six digits of the pay item are not used.

SPECIAL NOTE

Survey Work for Sidewalks and Curb Ramps

The contractor shall be responsible for field verifying all elevations and dimensions to ensure that the final layout of sidewalks and curb ramps meet ADA requirements prior to pouring concrete or placing asphalt or pavers. The survey work necessary to meet these requirements shall be included in the cost of Item 625.01 - *Survey Operations*.

SPECIAL NOTE
TEMPORARY LANE/SHOULDER CLOSURE RESTRICTIONS FOR MAJOR HOLIDAYS

There shall be no temporary lane/shoulder closures on roadway facilities owned and/or maintained by NYSDOT on the major holidays listed below.

Construction activities that will result in temporary lane/shoulder closures shall be suspended to minimize travel delays associated with road work for major holidays as follows:

Holiday	Falls on	Temporary lane closures are NOT allowed from
New Year's Day Independence Day Christmas Day	Sunday or Monday	6:00 AM Friday before to 6:00 AM Tuesday after
	Tuesday	6:00 AM Saturday before to 6:00 AM Wednesday after (starting at 6:00 AM Friday before to 6:00 AM Wednesday after for Christmas Day)
	Wednesday	6:00 AM Tuesday before to 6:00 AM Thursday after (starting at 6:00 AM Saturday before to 6:00 AM Thursday after for Christmas Day)
	Thursday	6:00 AM Thursday to 6:00 AM Monday after (starting at 6:00 AM Wednesday before to 6:00 AM Monday after for Christmas Day)
	Friday or Saturday	6:00 AM Thursday before to 6:00 AM Monday after

Holiday	Falls on	Temporary lane closures are NOT allowed from
Memorial Day Labor Day	Monday	6:00 AM Friday before to 6:00 AM Tuesday after
Thanksgiving Day	Thursday	6:00 AM Wednesday before to 6:00 AM Monday after

Exceptions can only be made under the following conditions:

- Emergency work.
- Work within long-term stationary lane/shoulder closures.
- Safety work that does not adversely impact traffic mobility and has been authorized by the Regional Traffic Engineer.

Note: The Department reserves the right to cancel any work operations, including lane closures and/or total road closures, that would create traffic delays by unforeseen events. The Contractor would be notified at least seven (7) calendar days prior to the proposed work.

SPECIAL NOTE
ADDITIONAL TEMPORARY LANE/SHOULDER CLOSURE RESTRICTIONS FOR
OTHER HOLIDAYS AND/OR SPECIAL
EVENTS

There shall be no temporary lane/shoulder closures on roadway facilities designated below on these additional holidays or special events.

Designated Roadway Facilities		
Facility	Limits	Holiday/Event
All	Contract limits	Christmas and New Year's
Henry W. Dubois Drive	Between street NY Route 32 and Church St.	Veteran's Day

Construction activities that will result in temporary lane/shoulder closures shall be suspended to minimize travel delays associated with road work on these additional holidays or special events as follows:

Holiday or special event	Falls on		Temporary lane closures are NOT allowed from
	Day	Date (mm/dd/yyyy)	
Veteran's Day	Saturday	11/11/2023	Beginning 6:00 AM Friday, November 10 and ending 6:00 AM Sunday, November 12
Christmas and New Year's	Both Monday	12/25/2023 and 01/01/2024	Beginning 6:00 AM Friday, December 22 and ending 6:00 AM Tuesday, January 2

Exceptions can only be made under the following conditions:

- Emergency work.
- Work within long-term stationary lane closures.
- Safety work that does not adversely impact traffic mobility and has been authorized by the Regional Traffic Engineer.

SPECIAL NOTE

ULTRA LOW SULFUR DIESEL FUEL

In order to reduce diesel emissions, the Contractor shall use Ultra Low Sulfur Diesel (ULSD) fuel to operate all diesel engines used to complete the work that will operate for 10 hours or more on the contract site. ULSD fuel requirements shall apply to:

- All diesel engines/equipment
- Stationary and mobile equipment
- Owned, leased and rented equipment.

The hours the piece of equipment is used to complete the work is defined as the actual time the engine is running. The time may be continuous or discontinuous and includes warm-up periods idling, in traffic periods, etc.

The term "Contractor" is intended to mean both Prime Contractors and Subcontractors. Materials delivery vehicles not owned by the Contractor/Subcontractor are exempt from this requirement, but should minimize idling time at construction sites when ever possible.

The Contractor will be notified when any diesel powered construction equipment is in noncompliance. Non-compliance shall be corrected within a 24-hour period.

SPECIFICATIONS

A. SPECIFICATIONS

The State of New York Department of Transportation Office of Engineering *Standard Specifications*, including any addenda to date, are hereby incorporated, in their entirety, and made a composite part of these specifications except as herewith modified and supplemented by these specifications and subsequent documents issued by the Village of New Paltz.

The Specifications, Plans and Contract Documents of the Village shall govern over those of other agencies but where the method of work and requirements of materials are not included in the Contract Documents of the Village, the State of New York Department of Transportation *Standard Specifications* shall govern.

The Contractor should note that the Project Plans are dimensioned in US Customary (English) units only, and that all Payment Items will be measured and paid in standard US Customary (English) units.

The Engineer for the Village of New Paltz or their authorized representative shall make the final interpretations of any irregularities, ambiguities or questions arising out of these specifications and the New York State Department of Transportation *Standard Specifications* used on this project. References to the Village's representative and Village personnel shall hereinafter be made as the "Engineer".

B. DEFINITIONS

Whenever the words directed, required, permitted, ordered, instructed, designated, considered necessary, or where the words of like import are used, it shall be understood that the direction, requirement, permission, order, instruction, designation or prescription of the Engineer is intended; and similarly, the words approved, acceptable, satisfactory, or words of like import shall mean approved by or acceptable or satisfactory to the Engineer, unless another meaning is plainly intended. Whenever, in the description of any part of the Work to be done under this Contract, the expression as shown, as shown on the plans or the words of like import are used, it shall be understood to mean as shown on the Contract Drawings, unless another meaning is plainly intended.

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C. SPECIAL SPECIFICATIONS

The following Special Specifications are contained in this project:

Item	Description	Unit
608.21000003	CAST IRON EMBEDDED DETECTABLE WARNING UNITS	SY
627.50140008	CUTTING PAVEMENT	LF
680.81310109	ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITH POLE	EA
680.81310209	ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITHOUT POLE	EA

ITEM 608.21000003 - CAST IRON EMBEDDED DETECTABLE WARNING UNITS

DESCRIPTION

Section §608-1 of the Standard Specifications shall apply.

MATERIALS

Section §608-2.07 of the Standard Specification shall apply with the following modifications:

Embedded Detectable Warning Units 726-02

All embedded detectable warning units shall be cast iron. No other material will be accepted. Installation of detectable warning units shall be in accordance with manufacturer's recommendations. All detectable warning units shall have a natural finish color.

CONSTRUCTION DETAILS

Cast iron detectable warning units shall be installed in wet concrete as directed by the manufacturer. Follow all applicable manufacturer's requirements for environmental conditions, surface preparation, installation procedures, curing procedures, and materials compatibility.

METHOD OF MEASUREMENT

Section §608-4.07 of the Standard Specifications shall apply.

BASIS OF PAYMENT

The unit bid price per square yard shall include all labor, material, and equipment necessary to satisfactorily complete the work, including bedding material. No adjustment shall be made for concrete removed to accommodate embedded units.

Payment will be made under:

Item No.	Item	Pay Unit
608.21000003	Cast Iron Embedded Detectable Warning Units	Square Yard

ITEM 627.50140008 - CUTTING PAVEMENT

DESCRIPTION:

The contractor shall cut existing asphalt pavement, concrete pavement, asphalt surface course, or asphalt concrete overlay on concrete pavement at the locations indicated and detailed on the plans and as directed by the Engineer.

MATERIALS:

None specified.

CONSTRUCTION DETAILS:

Existing pavement and overlay shall be cut perpendicular to the roadway surface along neat lines, and to the depth indicated on the plans and typical sections, using appropriate equipment. After the pavement has been cut through, the Contractor may use pry bars, pneumatic tools or other methods, to pry loose the pavement to be removed from the pavement that is to remain. A pavement breaker may be used to break up the pavement to be removed after the pavement has been completely cut through and completely free from the pavement to remain.

When pavement cutting is called for in the Contract documents, if a neat vertical face with minimal shatter is obtained by performing an adjacent operation (such as milling) which eliminates the need to perform a separate pavement cutting operation, payment will be made for both the pavement cutting item and the item for the adjacent operation.

Any existing pavements and curbs not indicated to be removed that are damaged by the contractor's operations, shall be repaired at no additional cost to the State. Pavement cutting that the contractor chooses to do for his/her own convenience shall not receive any additional payment from the State.

METHOD OF MEASUREMENT:

The quantity to be measured will be the number of linear feet of pavement cutting satisfactorily completed.

BASIS OF PAYMENT:

The unit price bid per linear foot of pavement cutting shall include the cost of all labor, materials, and equipment necessary to satisfactorily complete the work.

Payment for prying, breaking, removal and disposal of cut pavement shall be made through other appropriate items.

ITEM 680.81310109 - ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITH POLE

ITEM 680.81310209 - ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITHOUT POLE

DESCRIPTION

The APS shall consist of a Central Control Unit (CCU) and Accessible Pedestrian Push Button Stations (PBS). This work shall consist of furnishing and installing a APS in accordance with the Contract Documents or as directed by the Engineer. The System shall meet the functionality requirements of MUTCD 2009-4E.

MATERIALS

The Accessible Pedestrian Push Button Station shall be ADA compliant. It shall contain all electronic control equipment, mounting hardware, Audible-Tactile push button and 9 inch by 15 inch informational pedestrian sign mount with bracket assembly-sign face (MUTCD # R10-3E). The Audible-Tactile push button shall be designed to provide both a button with a raised directional vibrating tactile arrow on the button and a variety of audible sounds for different pedestrian signal functions. The unit shall have a weatherproof speaker, and the appropriate informational sign for each location.

The system shall consist of a Control Unit and the Accessible Pedestrian Push Button Station with Pole Mounting Assembly.

The Systems Specifications

- Pole Unit Speaker with, microphone shall be located in the PBS, non-visible, environmentally protected housing
- Pole Unit Temperature Range: - 30°F to 165°F
- Pole Unit Push Button: ADA compliant with integrated sign bracket for the MUTCD # R10-3E sign
- Temperature and Humidity requirements- meet NEMA TS 2 Section 2.1
- Voltage Protection requirements - meet NEMA TS 2 Section 2.1
- Mechanical Shock and Vibration requirements - meet NEMA TS 2 Section 2.1
- Transient Suppression requirements – meet IEC 61000-4-4, ICC 61000-4-5
- Electronic Noise requirements – meet FCC Title 47, Part 15, Class A
- Electrical Reliability requirements- meet NEMA TS4 (Applicable Portions of Section 8)
- Enclosure requirements, (PBS) shall meet NEMA 250-Type 4X E, (CCU) shall meet NEMA 250-Type 1

ITEM 680.81310109 - ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITH POLE

ITEM 680.81310209 - ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITHOUT POLE

Audio and System Specifications

- Volume Control Automatic Adjustment Range: 28dB Max
- Microphone for Ambient Noise:
Approximate frequency range: 170 Hz to 2.3 kHz
- Button Tone: A brief “tick” confirms each button push
- Audible Locating Tone: 880Hz plus harmonic, 0.1 second duration, 1 second interval. Operates during pedestrian clearance and don’t walk interval.
- LED Operation: The LED lights when the button is pushed and remains lit until the next walk phase.
- LED Luminous Intensity:
Greater than 1200 mcd, sunlight visible, ultra bright red, viewing angle 160°
- System shall provide the following audible features:
 - A locating tone
 - 5 walk sound choices
 - 3 pedestrian clearance sound choices
 - Direction of travel
 - User programmable informational message
 - Audible sound must emanate from push button
- System shall provide a “Wait” message that plays once the button is activated.

CONSTRUCTION DETAILS

The Control Unit shall be mounted in the pedestrian head and powered from the pedestrian head lamp indications. The Pole Mounting Assembly shall be mounted on a pole near the start of the crosswalk. The Pedestrian push button shall be mounted between 3 ft. and 3 ft. 6 in. above an accessible surface and shall face an accessible approach (orientated parallel to the line of pedestrian travel) and within 10 in of adjacent accessible surface, as per Standard Sheets “Sidewalk Curb Ramp Details” and as per Standard Sheet “Pedestrian Signals and Flashing Beacon Installation Details”. All installations shall conform to manufacturers specifications and details required for a complete working Accessible Pedestrian Push Button. Street name shall be programmed, if there is an adjacent push button within 10 feet or as directed by the Engineer.

METHOD OF MEASUREMENT

This work will be measured by the number of stations satisfactorily furnished and installed.

BASIS OF PAYMENT

The unit price bid for each Accessible Pedestrian Push Button Station installed shall include the cost of all labor, materials, equipment, system programming and testing necessary to complete the work.

SCHEDULE II: DEADLINE SCHEDULE

A.	Publication of Notice:	April 12, 2023
B.	Submission of Proposals:	May 2, 2023
C.	Evaluation of Proposals:	May 10, 2023
D.	Contingent Award:	May 24, 2023
E.	Execution of Contract with Notice to Proceed:	June 1, 2023
F.	Project Completion:	December 22, 2023

The Village of New Paltz reserves the right to modify this Deadline Schedule as necessary. All Submittals shall be submitted in hard copy, signed in the original, and received and date stamped by the Village of New Paltz on or before 11:00 a.m. on May 2, 2023.

Respondent is responsible for meeting all deadlines. Failure to meet this deadline may result in the Village of New Paltz exercising its right to terminate negotiations with the selected firm.

SCHEDULE III: INSURANCE REQUIREMENTS

In accordance with Standard Specifications §107-06A.4 applicable insurance policies shall be endorsed to provide coverage to:

- The State of New York / New York State Department of Transportation
- Any municipality in which the work is being performed
- Any public benefit corporation, railroad, or public utility whose property or facilities are affected by the work
- Any consultants working for or on the project
- Agents or employees of the above listed parties

As any new locations of work are defined or added to the Contract, the Contractor shall extend coverage to any new parties that warrant coverage as per §107-06A.4. Proof of coverage for the new additional insured parties shall be provided to the Department.

Coverage shall be extended to the following known additional insured parties:

- County of Ulster
- Village of New Paltz
- Verizon
- Central Hudson Gas & Electric
- Spectrum
- Alta Planning + Design, Inc.
- H+Z Engineering + Landscape Architecture, D.P.C.
- Control Point Associates, Inc.

SCHEDULE IV: NOT TO EXCEED COST

The following resolution adopted by the New Paltz Village Board of Trustees determines and sets a not to exceed dollar amount to which bidders are asked to adhere to. The lowest qualified bidder within the identified amount will be chosen.



VILLAGE OF NEW PALTZ BOARD OF TRUSTEES

Resolution No. ____ of 2023

The following was presented by _____ Sec'd by _____ Date of Adoption _____

Names	Ayes	Noes	Abstain	Absent
Mayor Rogers	X			
Deputy Mayor Wojcik	X			
Trustee Weisburd	X			
Trustee Wheeler-Murray	X			
Trustee Zipp	X			
Totals	5			

RESOLUTION OF THE VILLAGE OF NEW PALTZ BOARD OF TRUSTEES SETTING A CAPPED DOLLAR AMOUNT FOR BID PROPOSALS FOR THE HUDSON RIVER VALLEY GREENWAY FUNDED RAIL TRAIL ACCESS PROJECT

WHEREAS, The Village of New Paltz is seeking to provide a more direct route for Empire State Trail users to connect from the proposed Henry W. Dubois shared path to the existing Wallkill Valley Rail Trail (WVRT) at the newly reconstructed and signalized NY Route 32/Henry W. Dubois intersection; and

WHEREAS, This intersection improvement project at the intersection of NY Route 32 (N. Chestnut Street) and Henry W. Dubois Drive in the Village of New Paltz consists of the addition of one ADA compliant concrete curb ramp and reconstruction of one existing concrete curb ramp.

Additional improvements include pedestrian signals with countdown timers, curb work, and re-striping along NY Route 32 to accommodate a new crosswalk; and

WHEREAS, This Village project sponsored by the Hudson River Valley Greenway and the limited grant funding was intended to cover the entire cost of the project.

THEREFORE BE IT RESOLVED, That The Village of New Paltz is setting a capped dollar amount of \$120,000 for bid proposals; and

BE IT FURTHER RESOLVED That The Village of New Paltz intends to select the bidder with the lowest bid amount within the aforementioned identified cap of \$120,000.

The Resolution was thereupon adopted.

Nicole MacLean, Deputy Clerk

Date